



CITY OF KIRKLAND
Planning and Community Development Department
123 Fifth Avenue, Kirkland, WA 98033 425.587-3225
www.kirklandwa.gov

MEMORANDUM

To: Planning Commission

From: Angela Ruggeri, AICP, Senior Planner
Paul Stewart, AICP, Deputy Director
Eric Shields, AICP, Planning Director

Date: December 4, 2014

Subject: Parkplace Amendment Request
FILE # CAM14-02188

RECOMMENDATION

Receive a briefing on the proposed amendments to the zoning text for CBD-5A and the proposed modifications to the Master Plan and Design Guidelines for Parkplace. Review and discuss any additional information needed for the public hearing on January 29, 2015.

BACKGROUND DISCUSSION

The City Council received a letter dated October 3, 2014 from G. Richard Hill representing Kirkland Parkplace requesting the City consider changes to the Zoning Code for CBD 5A and revisions to the Master Plan and Design Guidelines. At its October 21, 2014 meeting, the City Council directed the Planning Commission to study and provide a recommendation on the Parkplace proposal to amend the zoning text for CBD 5A and on the requested modifications to the Master Plan and Design Guidelines. As discussed below, the most significant amendment requested is to increase the percentage of residential use allowed. On November 12, 2014, the Planning Commission received a briefing on the proposed amendments and discussed the work plan. At that meeting, the Planning Commission had a number of questions. Additional information relating to the requested amendments has been provided below.

A. Parkplace Current and Previous Proposal:

The Parkplace property (see Attachment 1) is now owned by KPP Development LLP and the owner has presented the City with its proposal to modify the zoning text for CBD 5A and the adopted Master Plan and Design Guidelines for Parkplace. The proposed project is expected to have approximately 1,140,000 square feet which is significantly less than the original 1,750,000 square feet in the previously approved proposal.

The existing zoning that was put in place in 2008 allows building height up to a maximum of 8 stories (up to 115 feet) on most of the site, with lower heights adjacent to Peter Kirk Park and Central Way. No changes are being proposed to the allowed height.

The Planning Commission recommendation for Parkplace that was made to the City Council in 2008 is included as Attachment 2 to this memo.

B. Process:

In the adopted Master Plan and Design Guidelines, major modifications to the Master Plan are required to be reviewed by staff for consistency with the Comprehensive Plan and to be approved by the City Council. KMC 3.30.040 states that the City Council shall consult with the Planning Commission prior to amending the Master Plan and Design Guidelines.

The Planning Commission will make recommendations to the City Council on the zoning text changes and amendments to the Master Plan and Design Guidelines. At the November 13 meeting the Commission requested to see the conceptual plans that the applicant had been showing to various groups. The Planning Commission will not be reviewing the actual design of the project. Section 160.60 of the Zoning Code states that the City may not consider a specific proposal site plan or project in deciding whether or not a proposal should be approved. Therefore, the preliminary conceptual plans that are included as Attachment 3 are for information only.

The project design will be reviewed by the Design Review Board (DRB) in 2015, after the City Council has made a decision on the proposed changes to the zoning text and the Master Plan and Design Guidelines. These documents will be used by the DRB in their review of the project design.

C. Proposed Zoning Text Changes:

The proposed changes to the zoning text that the Planning Commission will be reviewing include the following (see Attachments 4):

- The current zoning code limits residential development to 10% of the total gross floor area on the site; a zoning amendment is requested to increase this limit to 30%.
- The current code requires that the gross floor area of retail and restaurant uses be equal to or greater than 25% of the gross floor area of office uses in the zone. The current zoning also provides an incentive to include a movie theater in the project. The code states that a maximum of 10% of the required retail and restaurant square footage may be met by movie theater square footage. A zoning text amendment to change this incentive to 20% of the required retail and restaurant square footage has also been proposed.

- The current code does not allow drive through facilities. A zoning text amendment to allow a bank drive through is requested.
- Staff is proposing a 10% affordable housing requirement if the residential percentage is increased. This 10% of affordable housing is commonly required when residential development capacity is increased. The requirement is in accordance with Comprehensive Plan Policies H-2.3 and H-2.4 which state:

Policy H-2.3: Promote the provision of affordable housing by private sector residential developments.

Policy H-2.4: Provide affordable housing units when increases to development capacity are considered.

The following Zoning Code criteria must be met when zoning text changes are proposed.

135.25 Criteria for Amending the Text of the Zoning Code

The City may amend the text of this code only if it finds that:

1. The proposed amendment is consistent with the applicable provisions of the Comprehensive Plan; and
2. The proposed amendment bears a substantial relation to public health, safety, or welfare; and
3. The proposed amendment is in the best interest of the residents of Kirkland.

D. Comprehensive Plan:

Staff has determined that Comprehensive Plan amendments will not be necessary for this proposal. The Comprehensive Plan Chapter for the Moss Bay Neighborhood, Downtown Plan states that "Limited residential use should be allowed as a complementary use" within the Parkplace Center site along with retail and office uses. The exact Comprehensive Plan wording is as follows:

EAST CORE FRAME

Development in the East Core Frame should be in large, intensively developed mixed-use projects.

The East Core Frame is located east of Peter Kirk Park, extending from Kirkland Way northerly to 7th Avenue. The area includes the Kirkland Parkplace shopping center as well as several large office buildings and large residential complexes. South of Central Way, the area is largely commercial and provides significant opportunities for redevelopment. Because this area provides the best opportunities in the Downtown for creating a strong employment base, redevelopment for office use should be emphasized. Within the Parkplace Center site, however, retail uses should be a significant component of a mixed-use complex. Limited residential use should be allowed as a complementary use. The

north side of Central Way, within the East Core Frame, has been redeveloped to nearly its full potential with high density residential uses.

“Limited” is not defined in the Comprehensive Plan or the Zoning Code.

“Limited” is defined in the dictionary as “confined or restricted within certain limits.”

Similar situations where percentages are used in the Zoning Code are listed below for reference.

- Accessory Dwelling Units (ADU) (ZC Section 115.07) – permitted as “subordinate” use to a single-family dwelling. Square footage of detached ADU not to exceed the lesser of 800 square feet of gross floor area or **40 percent** of the primary residence and ADU combined.
- Light Industrial Zones (ZC Section 48.15.080) – banking or related financial services are allowed in the light industrial zones if “accessory” to the primary use and not more than **20 percent** of the gross floor area.

E. Proposed changes to Master Plan and Design Guidelines

Staff has been working with the applicant and their architect on documents showing proposed changes to the Master Plan and Design Guidelines. Drafts of these documents are provided in Attachments 5, 6 and 7. Attachment 5 is a summary of the key changes to the Master Plan & Design Guidelines. Attachment 6 is a copy of the 2008 Master Plan and Design Guidelines with proposed changes noted. Attachment 7 is a clean copy of the proposed Master Plan and Design Guidelines with changes incorporated.

The estimated square footages for uses in the new project are shown in the chart below and are compared to the approved 2008 Master Plan square footages. The differences between the approved 2008 Master Plan and the current proposal are also shown.

Use	2008 MP approval	Current proposal	Difference (2008 MP – Current Proposal)
Office	1,200,000 sf	600,000 – 615,000 sf	585,000 - 600,000 sf less in new proposal
Retail**/Fitness/ Entertainment/ Hotel	550,000 sf (includes a hotel)	210,000 – 225,000 sf (does not include a hotel)	325,000 - 340,000 sf less in new proposal
Residential	0	280,000 – 300,000 sf	Current proposal has 250-300 dwelling units.
Open Space (at grade)	160,000 sf	150,300 – 175,350 sf	Approximately the same
2 Rooftop Terraces	20,000 sf	20,000 sf	same

**The Zoning Code requires the gross floor area for retail and restaurant uses to be at least 25% of the total office square footage. This would be:

- 300,000 sf for the 2008 Master Plan
- 150,000 – 153,750 sf for the current proposal

A representative from Collins Woerman, the architectural firm for the project, will be available to present the key changes to the Master Plan and Design Guidelines at the Planning Commission meeting on December 11, 2014.

PUBLIC OUTREACH

Section 160.40.2 of the Zoning Code requires that a notice of the proposal and the public hearing be distributed at least 14 calendar days before the public hearing as follows:

- 1) The notice, or a summary thereof, will be published in the official newspaper of the City.
- 2) The notice, or a summary thereof, will be posted on each of the official notification boards of the City.
- 3) The notice will be posted on the City's website.

In addition to these requirements, the following notice will also be given.

- 3 public notice signs have been placed on the site.
- A notice has been emailed to all interested parties from the 2008 Parkplace list.
- A notice has been sent to the previous 2008 Parkplace listserv.
- There will be a mailing to the surrounding property owners and residents in advance of the public hearing.

A letter to the Planning Commission from a citizen, Margaret Bull, is included as Attachment 8.

SCHEDULE

An addendum to the Planned Action EIS and Supplemental Planned Action EIS that were done for the original Parkplace proposal is being prepared. The addendum will include updated traffic information. Revisions to the Planned Action Ordinance will also be necessary.

The following is a tentative schedule for the project.

01/29/2015 – Planning Commission review of EIS addendum, public hearing on the proposal and recommendation to the City Council.

02/17/2015 – City Council considers Planning Commission recommendation and takes action on the proposed Zoning Code and Master Plan amendments.

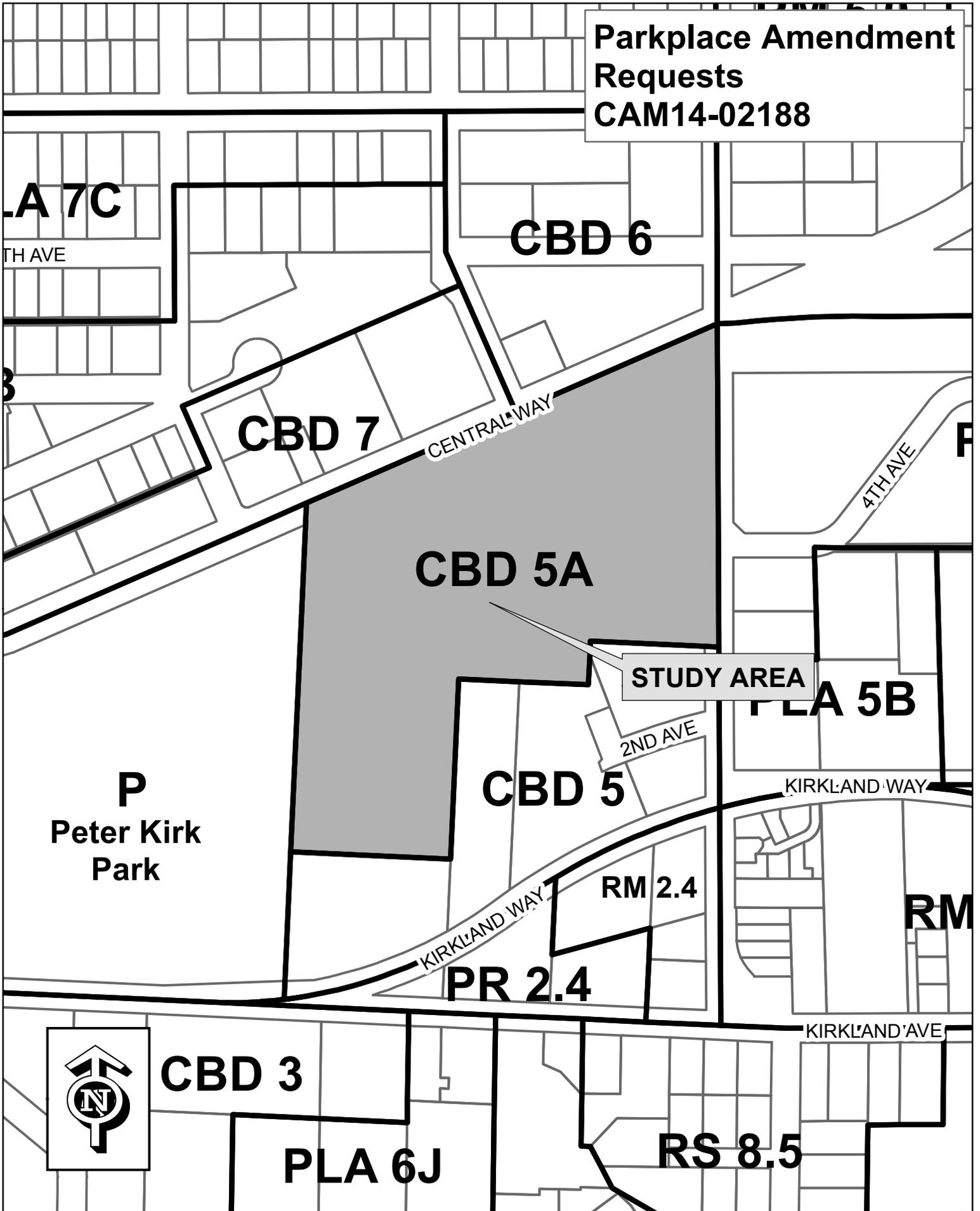
In addition, Parkplace representatives will be briefing the Park Board on 01/14/2015.

Attachments:

1. Study Area Map
2. 2008 Planning Commission Recommendation
3. Conceptual project plans
4. Use Zone Chart for CBD 5A
5. Summary of Key Changes to the Master Plan and Design Guidelines
6. 2008 Master Plan and Design Guidelines with proposed changes noted
7. Proposed Master Plan and Design Guidelines with changes incorporated
8. Letter from Margaret Bull to the Planning Commission

Cc: Rich Hill, Attorney for KPP Development LLP
Bill Pollard, Talon Private Capital
Jim Neal, Talon Private Capital
Joe Razore, MRM Kirkland, LLC
Moss Bay Neighborhood Association
KAN

**Parkplace Amendment
Requests
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MEMORANDUM

To: City Council

From: Planning Commission
Byron Katsuyama, Chair

Date: November 20, 2008

Subject: **PLANNING COMMISSION RECOMMENDATION
TOUCHSTONE (PARKPLACE) PRIVATE AMENDMENT REQUEST
FILE ZON07-00016**

I. INTRODUCTION

The Planning Commission is pleased to submit this recommendation on the Touchstone (Parkplace) Private Amendment Request. Touchstone has submitted a private amendment request (PAR) to amend the Comprehensive Plan and Zoning for the 11.5 acre site of the existing Kirkland Parkplace Center. The request includes a building height increase from 3-5 stories to 4-8 stories as measured from the grade of 6th Street and Central Way and allowance for taller buildings to be located next to Central Way and 6th Street. It also includes a request for a building setback reduction from 20 feet to 0 feet on Central Way and 6th Street, and from 10 feet to 0 feet next to Peter Kirk Park. Flexibility in other regulations such as parking requirements and lot coverage is also requested.

II. RECOMMENDATION ON THE TOUCHSTONE (PARKPLACE) PRIVATE AMENDMENT REQUESTS

The Planning Commission developed potential amendments to the Comprehensive Plan and Zoning Code, as well as a master plan and design guidelines after considering the Touchstone PAR over the past several months at study sessions and public hearings. In shaping draft codes and policies, the Planning Commission has met for over a year to weigh the benefits and impacts of the Touchstone PAR. The Planning Commission's recommendation reflects that deliberation, with the concept of using height as a tradeoff for public benefits including open space, sustainability measures, retail requirements, and pedestrian improvements. The Planning Commission has included regulations and design guidelines to enhance the relationship and orientation to Peter Kirk Park, create wider setbacks from adjoining properties, establish building step backs at key locations, and ensure light and sun into the central public open space. In the end, the majority of the Planning Commissioners supported these amendments, but there were two dissenting opinions which are summarized later in this memo. The recommended amendments include the following:

Comprehensive Plan

The draft amendments to the Downtown Plan include the following key revisions:

- East Core Frame Land Use District: Addition of retail as a significant part of any Parkplace mixed-use development and clarification of where and when residential uses are allowed.
- Urban Design: reference to a new design review document titled “Master Plan and Design Guidelines for Parkplace”, and emphasis on public views near I 405 rather than at 6th Street and Central Way.
- Height and Design District 5: Divides the district into a Design District 5 (properties in the southern portion) and Design District 5A (the Parkplace site). Policies for Design District 5 stay largely the same with minor text edits and clarifications. Design District 5A policies establish:
 - Height range of 3-8 stories with maximum heights allowed as a tradeoff for public open space and creation of a retail destination.
 - Emphasis on vehicular and pedestrian circulation, landscaping, and open space.
 - Special attention to building design and size at downtown gateway, along Central Way, and adjoining Peter Kirk Park.
 - Aggressive sustainability measures including green building, transportation demand management measures, and low impact development techniques.

Various other minor text edits and corrections to the Downtown Plan are included. Note that additional changes to the text of the Moss Bay Neighborhood Plan are recommended for the Orni and Altom PAR's.

Zoning Map and Zoning Code

The draft amendments to the Zoning Map and Kirkland Zoning Code include the following key revisions:

- Creation of a new CBD 5A zone covering the Parkplace center site with regulations that establish the following:
 - The primary allowed use is mixed-use development with office, retail, and restaurant uses. The square footage of retail and restaurant uses must be equal to or exceed at least 25% of the office square footage.
 - Development must be pursuant to the Master Plan and Design Guidelines.
 - Maximum heights are established in four height sub-districts: the center/southeast portion of the zone allowing 115' buildings with a maximum of 8 stories, the Central Way frontage portion allowing 100' buildings with a maximum of 7 stories along Central Way, the Peter Kirk Park frontage portion allowing 60' buildings with a maximum of 4 stories, and a transitional area between the Park portion and the center portion allowing 100' buildings with a maximum of 7 stories.
 - The minimum setbacks are 55' from Peter Kirk Park, 20' from properties to the south and east, and 0' from Central Way and 6th Street.
 - Big box retail (over 70,000 square feet) and drive through uses are prohibited.
 - Rooftop appurtenances are allowed to exceed height limits by 16' with a maximum 25% coverage of rooftops.

- Parking for mixed-use development using a shared parking method is allowed. Parking reductions through parking management and a transportation management program may be considered.
- Amendments to Chapter 142 (Design Review) to incorporate the Master Plan and Design Guidelines under design review authority.

Master Plan and Design Guidelines

The amendments include creation of a master plan and design guidelines, adopted by the Kirkland Municipal Code Section 3.30 that would provide an additional level of detail for reviewing redevelopment plans for the Parkplace center site. The document is divided into three key elements:

- A Policy Overview section establishing the vision, procedures, and design intent.
- A Master Plan Standards section establishing basic site planning requirements for amenities, retail frontages, pedestrian space, access points, and the interior street grid.
- A Design Guidelines section establishing detailed design standards for the site and buildings. The Design Guidelines are divided into four sub districts (Gateway, Central Way, Park Interface, and Retail/Office Hub) to respond to the surrounding context and site conditions.

III. RATIONALE FOR PLANNING COMMISSION'S RECOMMENDATION

It's been nearly a year and a half since the City Council directed the Design Review Board (DRB) and Planning Commission to begin a review of the Parkplace private amendment request (PAR) in July, 2007. The DRB subsequently held a series of six study sessions reviewing a variety of conceptual development plans and made their final recommendation to the Planning Commission on March 25, 2008.

Building upon the DRB's recommendations, the Planning Commission began its own series of study sessions and public hearings to discuss the PAR and to hear input from citizens. The environmental review process was also going on at this time and the draft environmental impact statement was issued in April, 2008. This was followed by several more study sessions and a public hearing resulting in the development of the Commission's preferred alternative to be analyzed in the Final EIS (FEIS) which was issued on October 16, 2008. Throughout our review process there has been significant community interest that has resulted in hundreds of email comments, letters, petitions and public testimony both in support and in opposition to the proposed project.

The final result of this extended process is the Commission's recommendation on the Comprehensive Plan, Zoning, Master Plan and Design Guidelines, and Planned Action Ordinance for Parkplace that we are now transmitting for your consideration and review.

Interest-Based Approach to Project Mitigation Issues

Touchstone's PAR is designed to accommodate their proposal for a 1.8 million sq. ft. mixed-use project that includes 1.2 million sq. ft. of office space and an additional 300,000 sq. ft. of retail. Other uses include a hotel and athletic club.

Throughout our review, the Planning Commission has sought to work with the developer using an interest-based approach aimed at striking a balance between Touchstone's interests in maintaining the parameters of their mixed-use program and the community's interests in addressing issues regarding building size, bulk and mass, and its affect on community character as well as the anticipated traffic and parking issues.

While the commission is generally in favor of the mixed-use project, we have had serious concerns about the size and massing of the buildings on the site just as the DRB did during their deliberations and as they expressed to us in their final recommendations. We have, in fact, agreed with most of the DRB's recommendations on this issue and have crafted recommendations calling for increased building setbacks, upper-story step backs and reduced building heights and mass particularly along Central Way and along the park edge. Our proposal for a three-story height limit immediately adjacent to Central Way actually reflects limitations contained in the city's current regulations for the CBD5 zone.

We also agree with the DRB in their judgment that the best location for the tallest buildings will be along the southern and eastern boundaries of the site. The sloping contours of this site offer a unique opportunity to realize the comprehensive plan's vision for locating a greater intensity of office and retail development here while minimizing some of the inevitable visual impacts on surrounding development.

We have been generally pleased with Touchstone's willingness and ability to creatively address our questions and concerns and to find ways to incorporate these into the project's design, including calls for increased building setbacks, upper-story step backs, height restricted zones along Central and along the park edge, a network of public open spaces, green rooftop terraces, sustainable building measures, and other design-related requirements, the sum total of which we believe have achieved a viable balance between the interests of the developer and the surrounding community.

The Importance of Retail

The commission favors an office/retail mixed-use development for a number of reasons. First and foremost has been our conclusion that a strong retail component should be an essential element of any redevelopment of the Parkplace site.

Most of those who have spoken in favor of the mixed-use project have done so on the basis of their desire to see a vibrant destination retail development in our downtown. Many have also spoken in support of a retail mix that includes a significant proportion of neighborhood convenience retail that will give residents the option of shopping in Kirkland as opposed to having to travel to Redmond or Bellevue for that purpose. For many, this is a simple matter of convenience, but having such options also has implications for other important community goals including reduced traffic generation, increased sales tax revenues and carbon footprint reductions.

Although the voices of Kirkland's youth were not heard in proportion to their numbers during our review process, we were impressed with the testimony of one young lady who took the time to come to two of our public hearings to express her support for the mixed-use proposal and to point out the need for more safe and fun places for teens to go and meet their friends in our downtown. We have also heard similar opinions from parents and adults who have worked with teens in our community.

Another aspect that has been frequently mentioned is the desire to have more "third places" in Kirkland where our residents can go to gather, be entertained and socialize in a variety of indoor and outdoor settings. The multiple public open spaces, restaurants, theater, health club, upper story terraces and other amenities that are included in the mixed-use proposal will add considerably to these types of third place opportunities in our downtown.

Finally, the mixed-use proposal has received broad support among downtown business and property owners, including the Downtown Association and the Chamber of Commerce, and employees who have consistently given us the same message: 5,000 new office workers coupled with a strong retail presence at Parkplace will provide a much needed boost to all of our downtown businesses. An economic impact analysis provided by one of Touchstone's consultants projected a potential 20% increase in sales revenues for businesses located within the downtown area as a direct result of the mixed-use project.

No Requirement for Retail in Current CBD5 Zone

While many have raised questions about the desirability of amending the Comprehensive Plan and Zoning provisions affecting this site, we are convinced that few, if any, would oppose amending the current CBD5 zoning to require that any future development include a certain percentage of retail uses.

Would we be looking at the same or a similar project now if the current zoning code contained a requirement for a fixed percentage of retail in the CBD5 zone? There are those who argue that we would not. But, implicit in that argument is the assumption that any retail requirement that might have been contained in the zoning code would have made economic sense for potential developers. In spite of our best intentions there is no guarantee that this will always be the case.

In fact, any developer considering a mixed-use project on this site would still have to make their own independent determination as to the economic viability of their plans in light of the allowed building heights and any set retail requirements as well as many other aspects of our zoning regulations that can and do affect such bottom line business decisions.

Such zoning and economic considerations will always be factors that developers will have to evaluate as a part of their business decision making process. To be effective, our zoning regulations must be reasonably cognizant of such basic business and market factors.

Office Use

The office component of Touchstone's mixed-use proposal at 1.2 million sq. ft. will of course constitute the single largest use on the site representing an increase of approximately 1.1 million sq. ft. of office space in the area over existing conditions that will transform Parkplace into the key employment focal point of downtown Kirkland.

Intensive office development at this location is in keeping with the land use and economic development elements of the City's Comprehensive Plan that encourage more in-city employment. The policies relating to the East Core frame encourage redevelopment in large intense mixed-use development, particularly office. The CBD5 zone is highlighted as one of the best areas in which to develop a vital downtown employment base.

The mixed-use project is also in keeping with statewide GMA goals to reduce urban sprawl by directing more development into existing urban areas where public facilities and services exist or can be provided in an efficient manner and to encourage economic development through the promotion, retention and expansion of businesses.

Many of the business owners who wrote in or spoke to the Planning Commission emphasized their support for more office space to accommodate the needs of growing businesses in Kirkland. We heard numerous accounts of businesses that have been forced to leave Kirkland as they outgrew their existing space and were unable to find suitable alternatives within the city.

Parking

Touchstone's mixed-use project proposal includes a request for a significant reduction in the parking spaces that would normally be required for each of the uses on the site. A strict application of the parking standards contained in the city's zoning code would call for approximately 5,157 spaces. Touchstone is requesting that this number be reduced to approximately 3,650 spaces. The reduction is based upon a plan that will allow some of the parking on the site to be shared by the different uses whose peak parking demand characteristics vary by the time of day and/or by day of the week. In addition the project will implement transportation demand management and parking management programs to encourage use of alternative transportation modes and more efficient use of the available parking to ensure that the total parking supply on the site will be adequate to meet the demand.

The Planning Commission agrees with the Parking Advisory Board's conclusion that the parking demand estimate for the Parkplace mixed-use project appears to be reasonable as well as the analysis of the peaking characteristics of the various uses by time of day. We note that the use of parking demand rates in the Institute of Transportation Engineers (ITE) *Parking Generation* (3rd Edition) as the basis for the analysis provides a very conservative estimate since these rates are derived primarily from free-standing suburban sites without mixed-uses that have free parking. In addition, in response to a suggestion by the Parking Advisory Board, Touchstone added 150 more parking spaces to provide a buffer during peak commercial parking periods and to reduce the amount of circulation by vehicles looking for parking.

Traffic

Questions about the amount of traffic that will be generated by Touchstone's mixed-use project have consistently been at or near the top of nearly everyone's list of issues and concerns including both project opponents and supporters. With employment estimates for the project topping 5,000 new jobs and with Touchstone's plan to establish Parkplace as a regional retail destination, the Planning Commission certainly agrees that an understanding of the project's potential traffic impacts and proposed mitigation measures are critical elements in the review of this project. We know that the project is likely to add significantly to traffic volumes and congestion in the city. The key questions are: (1) how much new traffic will be generated by the project? (2) what mitigation measures are proposed to deal with it? and (3) how effective will the proposed mitigation measures be as measured by the city's level of service standards?

To answer these questions the Planning Commission has relied primarily upon the analysis provided by Jones & Stokes, the city's traffic consultants. The applicant's implementation of a transportation management plan will also have an effect on traffic levels to the extent they are successful with measures that encourage employees to use alternative modes of transportation.

The Institute of Transportation Engineers *Trip Generation Manual*, which is based upon observed data, was used to estimate vehicle trip rates. Various adjustments to the vehicle trip rates were made using mode split assumptions that were based upon local census data and data collected from actual Kirkland businesses subject to commute trip reduction (CTR) requirements. The Planning Commission asked many questions regarding the consultant's assumptions and methodology and has been generally satisfied with the quality of the analysis and the validity of consultant's conclusions.

A key table presented by the consultant and city staff, "Evaluation of 2014 TIA Mitigation Intersections – PM Peak Hour LOS," compares the projected levels of service, unmitigated and mitigated, associated with the "no action" and "proposed action" scenarios for the 10 intersections for which adverse LOS impacts were identified for 2014 which is the year projected for full project build out. These are the ten intersections where the project related traffic volumes were high enough to trigger mitigation requirements under the city's Traffic Impact Analysis standards.

Evaluation of 2014 TIA Mitigation Intersections – PM Peak Hour LOS

ID	Intersection	Traffic Control ¹	Existing		No Action				Proposed Action			
					Unmitigated		Mitigated		Unmitigated		Mitigated	
			LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay
Southwest Subarea												
4	Central Way/Parkplace Driveway	Signal	C	20.2	F	>200	A	10.0	F	>200	C	21.3
105	Central Way/6th Street	Signal	C	30.9	-	-	-	-	F	96.3	D	39
109	NE 85th Street/114th Avenue NE	Signal	F	87.7	F	132.1	F	93.0	F	227.9	F	110.4
110	6th Street/4th Avenue	Signal	B	12.7	-	-	-	-	E	75.1	C	22.0
112	Kirkland Way/6th Street	Signal	C	22.3	-	-	-	-	F	231.0	C	23.6
128	Central Way/5th Street	TWS	E	48.2	-	-	-	-	E	66.2	D	38.7
129	Central Way/4th Street	TWS	E	48.3	F	82.4	C	18.1	F	119.0	C	21.3
169	6th Street/7th Avenue	AWS	B	13.7	-	-	-	-	F	86.7	E	42.6
Northwest Subarea												
211	Market Street/15th Avenue	Signal	C	23.0	-	-	-	-	F	153.3	B	15.9
East Subarea												
402	NE 85th Street/124th Avenue NE	Signal	E	67	-	-	-	-	F	81.0	E	78.4

1. Traffic control for mitigated conditions. AWS = All Way Stop; TWS = Two Way Stop (LOS/Delay shown for worst movement at TWS)
Source: Heffron Transportation, Inc. 2008

What is noteworthy here with respect to the proposed action is that while the “unmitigated” LOS for most of these intersections is at a level F or E, the “mitigated” LOS improves significantly with most of the intersections achieving an LOS of C or D. Only one intersection is projected to be at a “mitigated” LOS of F, and that intersection (NE 85th St. and 114th Ave. NE) happens to be the only intersection that is at an LOS of F under existing conditions as well. It is also instructive to compare the “mitigated” LOS for the proposed action with the LOS levels under existing conditions at these intersections. This comparison shows that three of the intersections are actually projected to achieve a “mitigated” LOS that is a grade higher than their LOS under existing conditions. Four of the intersections have the same LOS for the existing and “mitigated” conditions, and three of them are reduced by one LOS grade level each from existing to “mitigated” conditions.

As the DEIS points out, while the effects of additional vehicles on traffic congestion can be mitigated to varying degrees the actual increase in traffic volumes generated by the project may be considered a significant unavoidable adverse impact. Significant adverse impacts can also result if one or more mitigation measures are not implemented.

A review of the data in the above table suggests that while there will certainly be some significant unavoidable adverse traffic impacts associated with this project, the projected change in LOS for

the 10 intersections that triggered impact mitigation requirements will likely be within an acceptable range as compared to current LOS levels.

Touchstone's Alternative Proposal

In addition to their mixed-use PAR proposal, Touchstone has submitted a design review application for an alternative development plan that does not require a Comprehensive Plan amendment. The alternative proposal would include 1.2 million square feet of office with limited retail. Touchstone has indicated a preference for the larger mixed-use PAR proposal, but has consistently maintained that it is only feasible if all of the requested additional building heights and related square footage are approved. The alternative office development would only be built if the PAR is not approved. Touchstone offered its most detailed explanation for the program/financial requirements that drive the 5-8 mixed-use proposal in its "[Planning Commission Response Packet](#)" dated June 20, 2008, beginning on page 3.

Throughout this process the Planning Commission has been acutely aware of the large size of the project and the likely significant impacts it will have on the downtown and surrounding areas of the city. At the same time, we are in general agreement that the mixed-use project that includes a strong retail component will, on balance, provide greater benefits to the city than the applicant's alternative proposal to build what will essentially be an office park on the same site. Both proposals would result in developments that are significantly larger than the current Parkplace development.

It is worth noting that while we have heard many people express their opposition to the applicant's project as proposed, most of those same individuals have also stated that they do support the concept of an office/retail mixed-use approach on the site, albeit at a significantly reduced scale. In contrast, few individuals have spoken in favor of the alternative office park proposal.

Project Benefits vs. Impacts/Costs

The following list summarizes the Planning Commission's overall evaluation of the proposed project's anticipated community benefits relative to the anticipated impacts and costs.

Benefits:

- Leverage additional building height into greater public benefits on the site including: public open space, green building design and less surface parking.
- Enhanced retail activity on the site and resulting additional sales and property tax revenues will aid city's fiscal needs. Retail sales in particular are an important revenue component for all Washington cities that have few alternative revenue sources.
- Enhanced shopping opportunities and convenience for residents (reduced need for trips to other regional shopping centers).
- Enhanced employment base for economic development.

- Enhanced office and retail activity will provide a much needed economic boost to nearby downtown businesses.
- More concentrated employment and retail activity will contribute to regional anti-sprawl goals (GMA).
- Increased employment opportunities for residents that are located close in (GMA).
- Make the best use of the commercial zones that we have to avoid commercial creep.
- Development of a new north/ south street that breaks up the existing super block between 3rd St and 6th St.
- Create greater community building opportunities and places where people can meet and interact (more third places).
- Greater hotel and meeting space. We have a deficit of meeting space.
- Increased ridership will lead to improved Metro service for all Kirkland riders.
- An additional venue for free public events, like summer concerts
- Publicly accessible roof-top gardens – the 1st in the city.
- Improved streetscape on Central Way, the primary access route into downtown Kirkland.
- Visually dramatic building as gateway to downtown Kirkland.
- Retail and residential do not make a successful community. They are two legs of stool, but without the third leg – employment – Kirkland is a bedroom community which means more commuting traffic, more pollution, and less shared experiences in our town.
- Avoid the all office business park alternative. 1.2 million square feet of office is possible today, with no retail and no public benefits. Peak hour traffic impacts are (nearly) the same, view impacts are (nearly) the same.

Impacts/Costs:

- Bigger/taller buildings will alter character of CBD5 with a significantly more intensive development pattern which many project opponents feel will have an adverse affect on community character.
- Higher intensity development on this site will result in some declines in LOS at nearby intersections compared to the “no action” and “office only” alternatives.
- Increased shading of buildings to the north and east will occur with both the “office only” and FEIS reviewed alternatives with slightly more shading with the FEIS reviewed alternative.

- View blockage of properties located to the south and east will be significant.
- There is some risk that the project will result in some parking spillover into surrounding neighborhoods. There are safeguards included in the planned action ordinance calling for corrective action on the part of the developer but this will require some monitoring and enforcement action by the city.

IV. CURRENT COMPREHENSIVE PLAN AND ZONING

The subject property is located in the Moss Bay Neighborhood area of the Comprehensive Plan. Policies for development of the area can be found in the Downtown Plan section. The Downtown Plan designates the area as East Core Frame for land use purposes and encourages development in this area to represent a wide range of uses in several large, mixed-use projects. The Downtown Plan notes that this area represents the best opportunity for a vital employment base and should continue to emphasize office redevelopment over residential. The Downtown Plan also designates the area as Design District 5 and discusses maximum building heights of three to five stories, preservation of a sense of openness, and lower height toward the perimeter stepping up to the center of the district. The Plan encourages building orientation to Peter Kirk Park, emphasizes pedestrian and vehicular circulation, and the significance of the gateway at the northeast corner.

The subject property is zoned CBD 5 which allows buildings three to five stories in height and allows a variety of uses including retail, office, hotel, and limited residential. There are currently no requirements in the CBD 5 zone for retail uses.

V. PRIVATE AMENDMENT REQUEST CRITERIA

Criteria found in the Zoning Code must be considered when reviewing a private amendment request.

A. Factors for Consideration: KCZ 140.25 establishes that the City must take into consideration, but is not limited to, certain factors when considering a Comprehensive Plan Amendment. Below is a list of the criteria followed by staff analysis.

1. *The effect upon the physical, natural, economic, and/or social environment.*

The effects of the proposed amendment have been reviewed in detail by the Draft and Final Environmental Impact Statement (EIS) and have been considered in the drafting of the proposed amendment.

2. *The compatibility with and impact on adjacent land uses and surrounding neighborhoods.*

The proposed amendments have been reviewed in the EIS for compatibility with and impact on adjacent uses and surrounding neighborhoods and mitigations have been identified where incompatibilities or significant impacts were identified.

3. *The adequacy of and impact on public facilities and services, including utilities, roads, public transportation, parks, recreation and schools.*

Existing public facilities and services have been evaluated in the EIS and with the mitigating measures identified in the Planned Action Ordinance the public facilities and services are adequate to accommodate the proposed amendment.

4. *The quantity and location of land planned for the proposed land use type and density.*

The proposal is located in an area designated in the Comprehensive Plan as an Activity Area. The Activity Area is planned for high density uses with an emphasis on commercial uses surrounded by high density.

5. *The effect upon other aspects of the Comprehensive Plan.*

The proposed amendment has been reviewed in the EIS for consistency with other aspects of the Comprehensive Plan.

B. Criteria for Amending the Comprehensive Plan: KZC 140.30 establishes the criteria for evaluating a Comprehensive Plan Amendment. These criteria and the relationship of the proposal to them are as follows:

1. *The amendments must be consistent with the Growth Management Act.*

The amendment is consistent with the Growth Management Act, including the following planning goals (RCW 36.70A.020):

- Urban growth. Encourage development in urban areas where adequate public facilities and services exist or can be provided in an efficient manner. *Locating an employment base and a concentration of retail in Kirkland's Central Business District is consistent with this planning goal. The EIS evaluates adequacy of public services and facilities to serve the potential development and concludes that they are adequate.*
- Reduce sprawl. Reduce the inappropriate conversion of undeveloped land into sprawling, low-density development. *The Parkplace site presents an urban infill opportunity that can concentrate jobs and retail in an appropriate urban environment within a designated urban growth area.*
- Transportation. Encourage efficient multimodal transportation systems that are based on regional priorities and coordinated with county and city comprehensive plans. *The Parkplace site is within walking and distance of the existing and soon to be improved Downtown Transit Center and an existing concentration of downtown shops and services. The proposal includes transportation demand management measures to reduce SOV use as addressed in the EIS.*
- Economic development. Encourage economic development throughout the state that is consistent with adopted comprehensive plans, promote economic opportunity for all citizens of this state, especially for

unemployed and for disadvantaged persons, promote the retention and expansion of existing businesses and recruitment of new businesses, recognize regional differences impacting economic development opportunities, and encourage growth in areas experiencing insufficient economic growth, all within the capacities of the state's natural resources, public services, and public facilities. *The proposal presents a substantial economic development opportunity for the City of Kirkland in an area that has public services and public facilities to accommodate that development (see EIS).*

- Environment. Protect the environment and enhance the state's high quality of life, including air and water quality, and the availability of water. *The draft Comprehensive Plan language encourages aggressive sustainability measures including green building, low impact development, deconstruction, and transportation demand management.*
- Citizen participation and coordination. Encourage the involvement of citizens in the planning process and ensure coordination between communities and jurisdictions to reconcile conflicts. *To date, the proposal has undergone 16 months of intensive public process in community meetings, open houses, DRB review, City Council meetings, Planning Commission public meetings and public hearings.*
- Public facilities and services. Ensure that those public facilities and services necessary to support development shall be adequate to serve the development at the time the development is available for occupancy and use without decreasing current service levels below locally established minimum standards. *The proposal has been reviewed through the EIS for adequacy of facilities and services to support the development. With identified mitigations, the development would meet Kirkland's levels of service.*

2. *The amendments must be consistent with the Countywide Planning Policies.*

The amendment is consistent with the Countywide Planning Policies. Kirkland is within a designated urban growth area. The Policies state that land within Urban Growth Areas shall be characterized by urban development (LU-26). Downtown Kirkland is designated as an Activity Area in Kirkland's Comprehensive Plan consistent with the Countywide Planning Policies (FW-17). Policies encourage infill development that enhance community character and include a mix of uses (LU-69) and support open space and neighborhood commercial land uses within office rather than single purpose office parks (LU-74). Policies encourage urban areas characterized by superior urban design as defined locally (FW-25). Economic development policies encourage the retention and expansion of the economic base and a business climate that is supportive of business formation, expansion, and retention (ED-6).

3. *The amendments must not be in conflict with other goals, policies, and provisions of the Kirkland Comprehensive Plan.*

The amendment has been reviewed for consistency with the Kirkland Comprehensive Plan. The amendment is generally consistent with Downtown Plan policies encouraging high density employment and commercial use in CBD 5. With the mitigation measures identified in the EIS and Planned Action Ordinance the amendments would not be in conflict with the Comprehensive Plan.

4. *The amendments will result in long-term benefits to the community as a whole, and is in the best interest of the community.*

If the request is approved, the amendments will provide the long-term community benefit of establishing a significant employment base in downtown Kirkland and an opportunity to maintain the community-oriented aspects of the current Parkplace site as a local retail destination enhanced with improved public gathering spaces. Office development in this area meets the objective of the Downtown Plan by providing a significant increase in office square footage adjacent to the core area as a way to enhance the core area for retail and service businesses (page XD.D-4). The mixed use approach to the amendments also allows mutually supportive land uses on the same site and opportunities for shared parking. As noted in the Economic Development Chapter of the Comprehensive Plan,

“Mixed use development, when combined with multi-story structures, promotes a more compact and sustainable land use pattern and encourages walking and transit use to reduce dependence on automobiles.” (page VIII-10)

The amendments do involve additional mass and scale of buildings to accommodate the proposed density and mix of uses and the issue of scale has been a consistent community interest in the public process to date. The Community Character and Economic Development Chapters of the Comprehensive Plan acknowledge the need to balance growth and change with protection of community character. This balancing of community interests to create long-term benefits to the community as a whole is reflected in the proposed amendments, which allow taller buildings in conjunction with community amenities, sustainability measures, and design standards.

Additional assessment of community interests is located in the next section - C of this report.

- C. Criteria for Rezone:** KZC 130.20 establishes the criteria by which a legislative rezone must be evaluated. These criteria and the relationship of the proposal to them are as follows:

1. *Conditions have substantially changed since the property was given its present zoning or the proposal implements the policies of the Comprehensive Plan; and*

The current CBD 5 zoning and the Comprehensive Plan policy basis were established in the late 1980's and early 1990's. Conditions have changed substantially since the zoning was established, with adoption of the Growth Management Act, significant development in the CBD, and a greater City and regional focus on urban infill development and transit-oriented development. In addition, the rezone would implement the proposed policies of the Comprehensive Plan currently under consideration.

2. *The proposal bears a substantial relationship to the public health, safety, or welfare; and*

Much of the public comment around the proposal has focused on areas of character, traffic, retail importance, parking, views, setbacks, job growth, and open space. A review of those public welfare issues follows:

- **Character:** The character of the area will change with redevelopment of the Parkplace center under the existing five story zoning or the proposed eight story zoning. The draft codes, policies, and guidelines do include measures that seek to balance this additional development intensity with new requirements to protect Kirkland's unique character.
- **Traffic:** Traffic impacts have been identified and evaluated against City standards in the EIS and appropriate mitigating measures incorporated.
- **Retail:** Many comments have identified the importance of preserving the retail components of the existing Parkplace center; however, retail is not a required component of redevelopment under the existing zoning for the area. The proposed rezone would require a substantial retail component in any mixed use development. In addition, the Master Plan establishes guidance for community serving retail such as grocery and theater uses within a redevelopment.
- **Parking:** Parking for the preferred alternative has been evaluated thoroughly through the EIS. The proposed zoning text includes base parking requirements but allows shared use to make more efficient use of the parking. Working in tandem with proposed transportation demand management measure to reduce vehicle trip, the zoning also allows parking reductions to be considered based on a parking and transportation management programs.
- **Views:** Views have been considered and evaluated in the EIS and during the policy discussions with the Planning Commission. It should be noted that Kirkland's Comprehensive Plan specifically notes that the City does not protect private views (page IV-10) but does protect public scenic views

and view corridors. Existing and potential views from 6th Street and Central Way across the Parkplace site were evaluated and determined to be negligible currently and likely eliminated with any redevelopment of the area. The more significant view of the water and mountains as one drives from I 405 down 85th Street were also evaluated and found to be largely unaffected by the proposed rezone.

- **Setbacks:** While the initial PAR requested elimination of setback requirements, the proposed zoning would reduce setback to 0' on Central Way and 6th Street but would increase setbacks along the Park from 0' to 55' and increases setbacks from adjoining properties to the south and east from 0' to 20'.
- **Jobs:** Significant job growth may occur under any redevelopment of the area. Touchstone has submitted an alternative proposal for design review that would include a similar square footage of office use but would not include the mixed use components such as retail that are envisioned under the rezone.
- **Open space:** The policy basis for the rezone and additional height as established in the draft Comprehensive Plan amendments is to provide an incentive to the create a network of public open space around which is organized a dynamic retail destination. The proposed rezone and supporting documents would create such an incentive and establish clear requirement.

Based on the mitigations incorporated into the Planned Action Ordinance, the restrictions and requirements incorporated into the CBD 5A zone, and the development requirements included in the Master Plan and Design Guidelines, the proposed rezone does bear a substantial relationship to the public welfare.

3. *The proposal is in the best interest of the community of Kirkland.*

There is clearly a diversity of community opinion around whether the proposed rezone is in the best interests of the community. The proposed rezone does provide a significant opportunity for the community to create a strong employment base in the downtown activity area and derive the economic development benefits that accompany that base. The proposed rezone provides an opportunity to rewrite the rules for redevelopment of the area to require the retail and open space amenities that the community has identified as valuable. As noted above, the proposed rezone has sought to identify the interests of the community and address them in a substantive way.

VI. TWO DISSENTING OPTIONS ON THE PLANNING COMMISSION

One Planning Commissioner felt that the permitted building height for the southwest corner of the site was too great and that a building of that size would not integrate well with the surrounding neighborhood and the park. The Commissioner felt that maximum height for this area of the site should not exceed 5 stories in order to avoid walling off adjacent development from the rest of the downtown and the park. He stated that he generally liked the project including the retail, orientation to the park, and the underground parking, but could not support the Planning Commission recommendation because of the 7 story height allowance for the southern portion of the site.

The other Planning Commissioner had problems with the design in general and felt that it did not fit the character of Kirkland. This Commissioner felt that 5 stories with retail was the appropriate design for the site.

VII. PUBLIC COMMENT

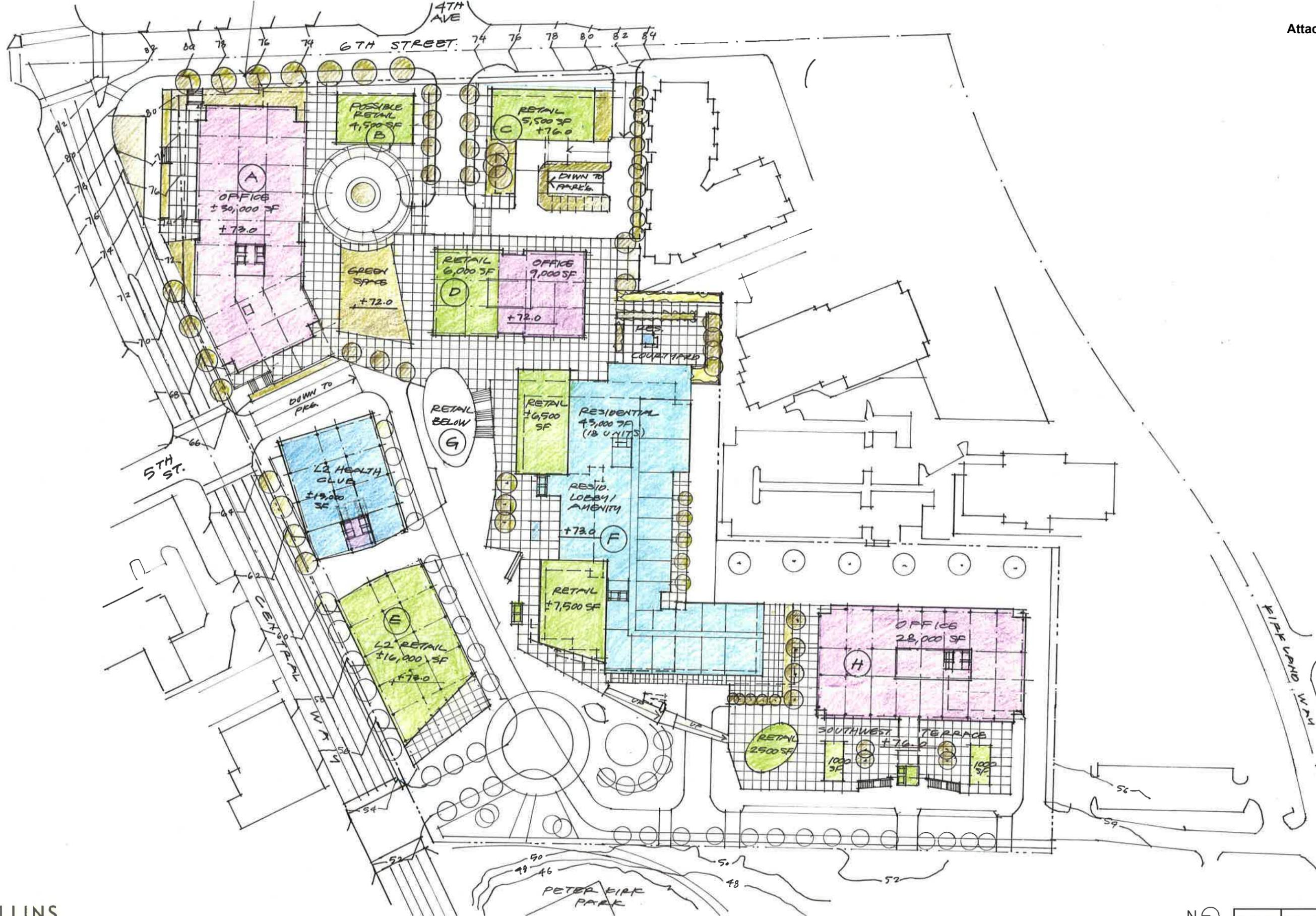
The Planning Commission has held three public hearings on the Touchstone PAR. The final public hearing prior to the Planning Commission making their recommendation occurred on October 23, 2008. Over the course of the year, they have also received hundreds of e-mails and letters along with petitions both for and against the proposal. Much of this correspondence has already been forwarded to the City Council. A full record is included in files in the Council Study Room for easy reference.

Those supporting the Touchstone private amendment request most often cited:

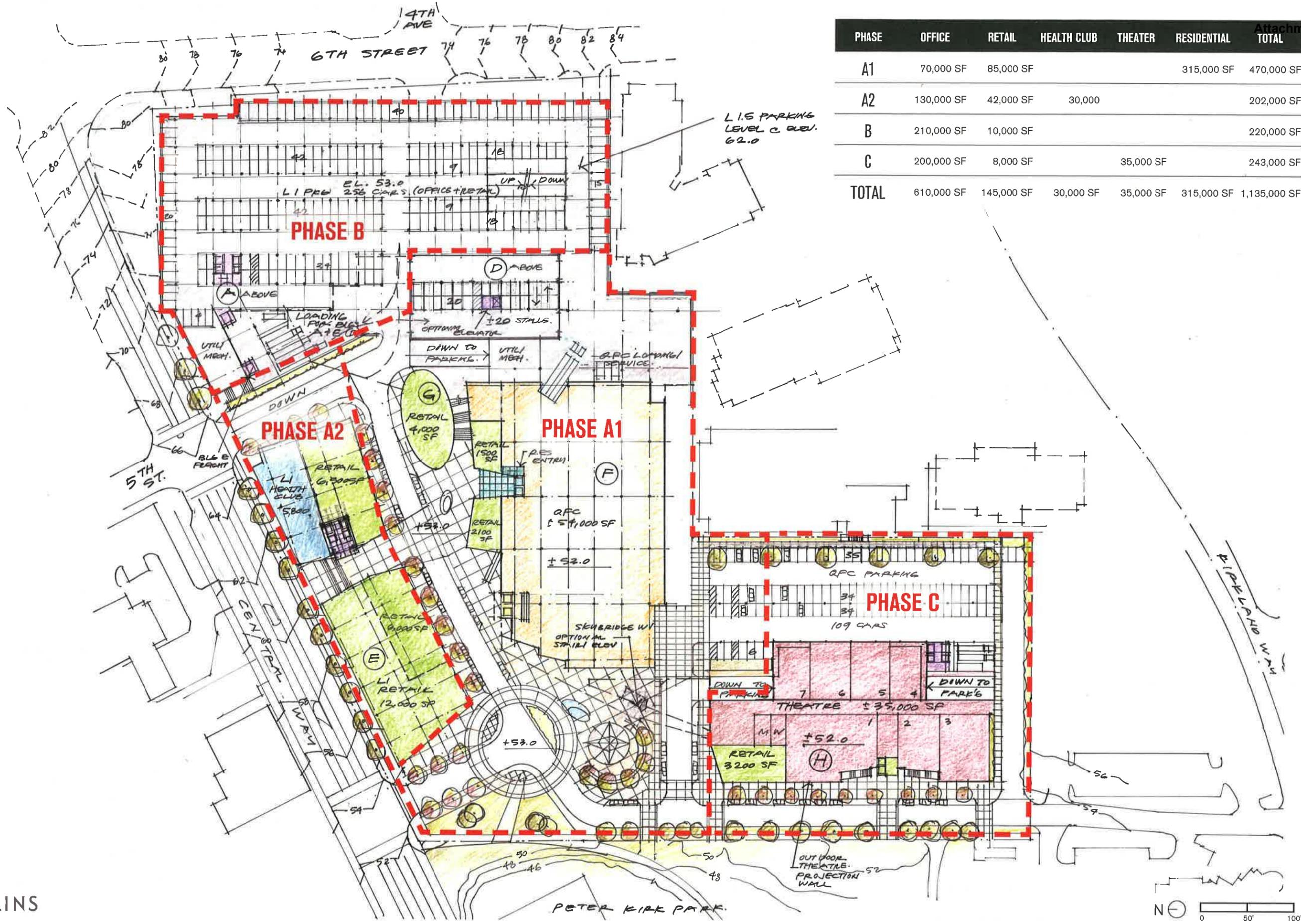
- Importance of retail in the project
- Growth in number of jobs
- Public open space
- GMA goals

Those against the project cited:

- Overall project size/height
- Traffic
- Parking
- Character of Kirkland
- Impacts of height, scale and building mass on properties to the east



PHASE	OFFICE	RETAIL	HEALTH CLUB	THEATER	RESIDENTIAL	TOTAL
A1	70,000 SF	85,000 SF			315,000 SF	470,000 SF
A2	130,000 SF	42,000 SF	30,000			202,000 SF
B	210,000 SF	10,000 SF				220,000 SF
C	200,000 SF	8,000 SF		35,000 SF		243,000 SF
TOTAL	610,000 SF	145,000 SF	30,000 SF	35,000 SF	315,000 SF	1,135,000 SF



PHASE	OFFICE	RETAIL	HEALTH CLUB	THEATER	RESIDENTIAL	TOTAL
A1	70,000 SF	85,000 SF			315,000 SF	470,000 SF
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B	210,000 SF	10,000 SF				220,000 SF
C	200,000 SF	8,000 SF		35,000 SF		243,000 SF
TOTAL	610,000 SF	145,000 SF	30,000 SF	35,000 SF	315,000 SF	1,135,000 SF



50.36 User Guide – CBD 5A zones.

The charts in KZC 50.38 contain the basic zoning regulations that apply in the CBD 5A zones of the City. Use these charts by reading down the left hand column entitled Use. Once you locate the use in which you are interested, read across to find the regulations that apply to that use.

<p>Section 50.37</p> 	<p>Section 50.37 – GENERAL REGULATIONS</p> <p>The following regulations apply to all uses in this zone unless otherwise noted:</p> <ol style="list-style-type: none">1. Refer to Chapter 1 KZC to determine what other provisions of this code may apply to the subject property.2. See KZC 50.62 for additional building height provisions.
---	---

link to Section 50.38 table



3. Developments creating four or more new dwelling units shall provide at least 10% of the units as affordable housing units as defined in Chapter 5 KZC. See Chapter 112 KZC for additional affordable housing incentives and requirements.



USE ZONE CHART

DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS

Section 50.38	USE REGULATIONS	MINIMUMS		MAXIMUMS		Sign Category (See Ch. 95)	Landscape Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)	Special Regulations (See also General Regulations)
		Required Review Process	Lot Size	REQUIRED YARDS (See Ch. 115)	Lot Coverage				
.010	Mixed Use Development Containing Office, Retail and Restaurant Uses. See Spec. Regs. 1, 2, 3 and 4.	D.R., Chapter 142 KZC. See Spec. Reg. 1.	None	See Plate 5, Chapter 180 KZC.	100%	None	See Spec. Reg. 6.	See Spec. Reg. 7.	<p>1. Development under this use shall be pursuant to the Parkplace Master Plan and Design Guidelines contained in Chapter 3.30 KMC. Compliance with the Master Plan and Design Guidelines shall be determined through DR, Chapter 142 KZC.</p> <p>2. The gross floor area of retail and restaurant uses in this zone shall be equal to or greater than 25 percent of the gross floor area of office uses in this zone. Retail uses may include accessory short term drop-off children's play facilities.</p> <p>3. The following additional uses are allowed subject to restrictions listed:</p> <ul style="list-style-type: none"> a. Hotel or Athletic Club. Accessory retail or restaurant uses shall be included as retail uses under Special Regulation 2, provided they are open to the public. b. Movie theater. This use may be included as a retail use under Special Regulation 2, provided, that the gross floor area of this use shall not count toward more than 10 percent of the required minimum gross floor area of retail and restaurant uses. c. Private Lodge or Club; Church; School, Day-care Center, or Mini-School or Day-care Center; Public Utility, Government Facility, or Community Facility; Public Park. d. Assisted Living Facility (including a nursing home if part of the facility); Stacked or Attached Dwelling Units; provided, that the gross floor area of these uses does not exceed 10 percent of the total gross floor area for the Master Plan. <p>4. The following uses are prohibited:</p> <ul style="list-style-type: none"> a. Any retail establishment exceeding 70,000 square feet. b. At grade drive-through facilities. c. The outdoor storage, sale, service and/or rental of motor vehicles, sailboats, motor boats, and recreational trailers. <p>5. Rooftop appurtenances may exceed the applicable height limitation by a maximum of 16 feet if the area of all appurtenances and screening does not exceed 25 percent of the total area of the building rooftop. All other regulations for rooftop appurtenances in Chapter 115 KZC shall apply.</p> <p>6. Prior to installation of permanent signs, the development must submit and receive approval of a Master Sign Plan pursuant to Chapter 100 KZC.</p>

20%

provided, that the gross floor area does not exceed 10 percent of the total gross floor area for the Master Plan; and

30%

unless designed & located not to interfere with pedestrian orientation & traffic circulation.

REGULATIONS CONTINUED ON NEXT PAGE

Section 50.38

Zone
CBD-5A

USE ZONE CHART

Section 50.38		DIRECTIONS: FIRST, read down to find use... THEN, across for REGULATIONS									
		Required Review Process	Lot Size	MINIMUMS			MAXIMUMS		Landscape Category (See Ch. 95)	Sign Category (See Ch. 100)	Required Parking Spaces (See Ch. 105)
REQUIRED YARDS (See Ch. 115)	Front			Side	Rear	Lot Coverage	Height of Structure				
.010	Mixed Use Development Containing Office, Retail and Restaurant Uses (continued)										REGULATIONS CONTINUED FROM PREVIOUS PAGE 7. The following establishes the number of parking spaces required: a. Residential uses must provide 1.7 parking spaces for each dwelling unit and one parking space for each assisted living unit. b. Restaurants and taverns must provide one parking space for each 125 square feet of gross floor area. c. All other uses must provide one parking space for each 350 square feet of gross floor area. A mix of uses with different peak parking times makes a project eligible for applying a shared parking methodology to parking calculations. Further parking reductions may be appropriate through a transportation management plan (TMP) and parking management measures. The development may propose and the Planning Official may permit a reduction in the required number of parking spaces based on a demand and utilization study prepared by a licensed transportation engineer. The study shall include an analysis of shared parking demonstrating that the proposed parking supply is adequate to meet the peak parking demand of all uses operating at the same time. A TMP and parking management measures shall be incorporated into the analysis. An analysis of the effectiveness of the TMP and parking management measures shall be provided for City review. The City's transportation engineer shall approve the scope and methodology of the study as well as the effectiveness of the TMP and parking management measures.

THE CITY OF KIRKLAND

Kirkland Parkplace Mixed-Use Development

Master Plan and Design Guidelines

Appendix II: DRAFT

Summary of Key Changes

from Existing 2008 Version to Proposed New Version

Prepared by CollinsWoerman

December 3, 2014

SECTION New Version	PAGE # New Version	PAGE # 2008 Version	CHANGE IN PROPOSED NEW DOCUMENT FROM 2008 VERSION
1	3	PO-3	Reduce: Development area from 1.75 million s.f. to 1.14 million s.f. Replace: Hotel and sports club uses with residential and entertainment uses. Add note for option of Property Owner to re-name the site.
5	4	PO-4	Add clarification: Planning Director reviews minor modifications.
6	4	PO-4	New phasing zones. Add: text to ensure no "rough edges" at the completion of each phase, and that demonstration of this requirement will happen during Design Review.
7	5	PO-3	This section in the new document contains information that was included in the 2008 document in Section 1. Add: Comprehensive Plan policy regarding Residential Use and corresponding Master Plan response.
8	6	PO-5	Add: "Provide connections to Peter Kirk Park" Add: Guiding Principle #9: <i>Mixed Use Development</i> to reflect addition of Residential component.
9.C	7	MP-6	Replace: green building requirements for hotel with green building requirements for residential (LEED for Homes: Multifamily Mid-rise; or Built Green 4 Star).
9.E	7	N/A	Add: parking program requirement: majority of parking is underground with surface parking at select locations to support retail uses.
9	8	MP-6	Revise: Area Breakdown Chart, including Site Area Breakdown, Open Space Breakdown (plazas and courtyards are combined for simplicity), and Building Gross Square Footage Breakdown. See chart for details.
10.A	8	MP-7	Revise: Pedestrian Connection diagram to reflect current development scheme. Add: note that owner shall work with the City to define appropriate wayfinding strategies between the development and the Cross Kirkland Corridor.

[continued]

SECTION New Version	PAGE # New Version	PAGE # 2008 Version	CHANGE IN PROPOSED NEW DOCUMENT FROM 2008 VERSION
10.B	9	MP-7	Remove: Retail Frontage diagram. The content of the 2008 version's Retail Frontage diagram is partially folded into the new <i>Organization of Uses</i> diagrams. See Section 10C. Retail is no longer anticipated at northeast corner of site. (See changes noted for 13.A: <i>Gateway District Design Guidelines</i> .)
10.C	9-11	N/A	Add New Section: "10.C. <i>Organization of Uses</i> " describes overall layout of site including: building uses, vehicular and pedestrian access and circulation, parking, and pedestrian space.
10.D	12	MP-7	Revise: Pedestrian Space diagram reflects new development plan. Replace: Atrium Breezeway requirement with requirements for Main Street Plaza, Upper Plaza, and Northwest Entry Garden. Atrium Breezeway no longer included due to wider spacing of office buildings. Add: further definition of particular pedestrian spaces.
11	13-17	MP-8-14	New street sections correspond with new street layout. See street classification and street section diagrams for details.
12	20	DG-17	Add: Design Guideline for graceful grade transitions.
12	23	DG-19	Remove: transparency requirement for 60% of facade facing Park Promenade due to possible entertainment uses in this location. Change: Retail height to 14' minimum, from 14'-18'. (It is anticipated that some retail/entertainment heights will be taller than 18'-0".)
12	24	DG-20	Remove: diagram of key locations for visual anchors.
13.A	26	DG-23	Replace: <i>Atrium Breezeway</i> with <i>Pedestrian Connection</i> .
13.A	27	DG-24	Replace: retail/restaurant uses at ground floor at Gateway District with more general "active and inviting" requirement. It is anticipated that office or office-related amenity uses would occupy the majority of the northeast corner. Replace: Upper level step back requirements at Gateway District with building form modulation requirement.
13.B	28-29	DG-25	Replace: "Storefront and hotel entrances" along Central Way with "Visibility into Buildings". No hotel is planned. Retail will have primary access from internal street. New Guidelines for reducing apparent bulk of buildings along Central Way: Replace step back requirement with: two-story pedestrian-only pass-through of buildings along Central Way; shifts or angles in building mass at upper stories; upper floor step back at western portion of office building.
13.C	30	DG-27	Add: planting zone and/or canopy at edge of buildings facing Peter Kirk Park where transparency is not feasible (due to potential entertainment uses). Add: retail/food service at rooftop terraces.
13.D	31	DG-28	Primary Plaza location shifts to western area of site, so name "Central Plaza" is no longer used. For this reason, language that referred to "Central Plaza" in 2008 version has been replaced with "Primary Plaza" or "Plazas" generally.
13.D	31	DG-28	The east-west pedestrian connection at the southern portion of site is not required to pass through a building. Instead, there is a requirement for pedestrian weather protection and wayfinding signs along the pedestrian connection.

THE CITY OF KIRKLAND

Kirkland Parkplace Mixed-Use Development

Master Plan and Design Guidelines

Appendix I: Roadmap to Changes

Prepared by CollinsWoerman
December 3, 2014

Purpose of this Appendix

The newly proposed *Kirkland Parkplace Mixed-Use Development: Master Plan and Design Guidelines* will replace the earlier document with the same name created in 2008. Much of the structure and content of the 2008 version continues to be relevant and is either left unchanged or is modified to meet the current design intent in the new document.

To help reviewers understand the differences between the two documents, this Appendix is provided as a “road map”. Changes are noted here and, where relevant, explanations are included.

Appendix II: Summary of Key Changes is a list for quick review of all changes of significant content, and complements this Appendix.

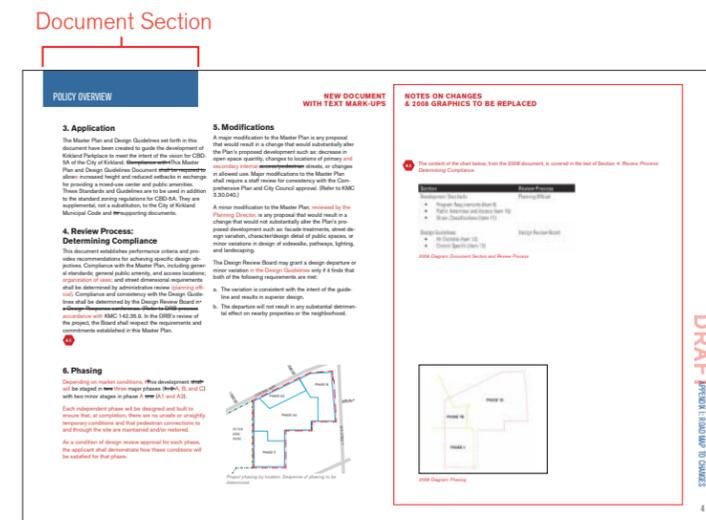
Change in Overall Format: The format of the new document (portrait-oriented, with two columns) is updated to be consistent with other Kirkland Design Guideline documents, such as *Design Guidelines for Yarrow Bay Business District* and *Design Guidelines for Pedestrian-Oriented Business Districts*.

Change in Images: All graphics in the document (diagrams, drawings, and photos) have been replaced.

Appendix Structure

This Appendix contains each page of the newly proposed document along with notes and portions of the old document for comparison. The following diagram illustrates a sample page of this Appendix.

Note: the layout of the final document is altered slightly in this appendix in order to fit crossed out text onto the page.



Content of New Document
including mark-ups of
2008 version text

Notes on Changes
and diagrams from 2008
version for comparison

Text Color Key:

Black: Text from the existing 2008 document.

~~Text with strike through~~: Text which is removed from the existing document appears in this appendix as crossed out.

Red: New text that was not in the 2008 document.

Keynotes:

Some notes are added to further clarify changes between the 2008 and the proposed documents.

7.1 Example Keynote for Section 7, note #1 of that section.

NEW DOCUMENT
WITH TEXT MARK-UPS

NOTES ON CHANGES
& 2008 GRAPHICS TO BE REPLACED

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Acknowledgment: Written content in this document has been excerpted and/or excerpted and edited from the previously approved *Kirkland Parkplace Mixed Use Development Master Plan and Design Guidelines* document updated December 16, 2008 as created by LMN Architects.

POLICY OVERVIEW

1. Introduction

Located along Central Way and 6th Street, Kirkland Parkplace has the potential to offer many great amenities to Kirkland's downtown. **Parkplace is a 501,000 square-foot property defined as CBD-5A in Kirkland's Zoning code.** The proposed mixed-use center includes approximately ~~1.75~~ **1.14** million square feet of development consisting of retail, office, ~~hotel and sports club~~ **residential, and entertainment uses** that are, in effect, an extension of the existing downtown. ~~Parkplace provides components that meet the City's Comprehensive Plan, East Core Frame (2004, 2008) as described below:~~
~~CP Policy: ...[additional text]~~

1.1

PURPOSE

This document includes three major parts: (1) a Policy Overview that establishes a vision, procedure, and design intent; (2) a Master Plan comprised of Development Standards that establish basic programming and site planning requirements; and (3) Design Guidelines that establish detailed design standards for the site and buildings.

These Standards and Guidelines provide structure to help meet the goals outlined in the Comprehensive Plan. A discussion of relevant Comprehensive Plan directives and this document's associated responses can be found in Section 7: *Comprehensive Plan Design Direction*.

PROJECT NAMING

While this document references the site's current name of "Kirkland Parkplace", the property owner may choose to re-brand the development and re-name it to reflect its new brand identity.

2. Vision

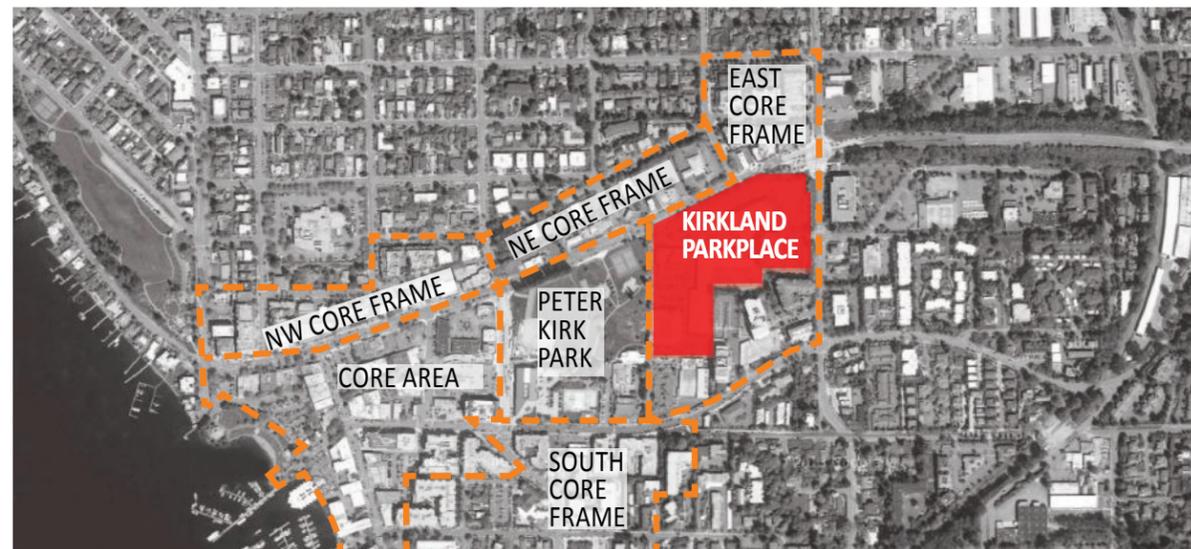
The Kirkland Parkplace Master Plan envisions a transformation of the existing suburban style office park and retail area to a lively, integrated mixed-use center.

Parkplace creates a new destination in Kirkland featuring tree-lined streets, landscaped open spaces, offices **and residences** overlooking public plazas, and a wide variety of shopping, dining, entertainment, and recreation experiences. Parkplace's contemporary Northwest architecture evokes Kirkland and its environs with green design, appropriate massing, and orientation. Appropriate placement of trees, fountains, benches, street lamps, and decorative sidewalk treatments add a rich texture to Parkplace's plazas and streets.

The combination of pedestrian-oriented streets, distinctive architecture, unique urban character, sensitive integration and progressive sustainable design strategies will make Kirkland Parkplace an attractive and valued gathering place for Kirkland's citizens **for years to come.**

The compact design includes a diversity of spaces for gathering and bustling activity, while maintaining a human scale. This reflects and celebrates the evolution of Kirkland: balancing the need for growth and economic opportunity, but not losing touch with the comfortable, small-town roots of its past.

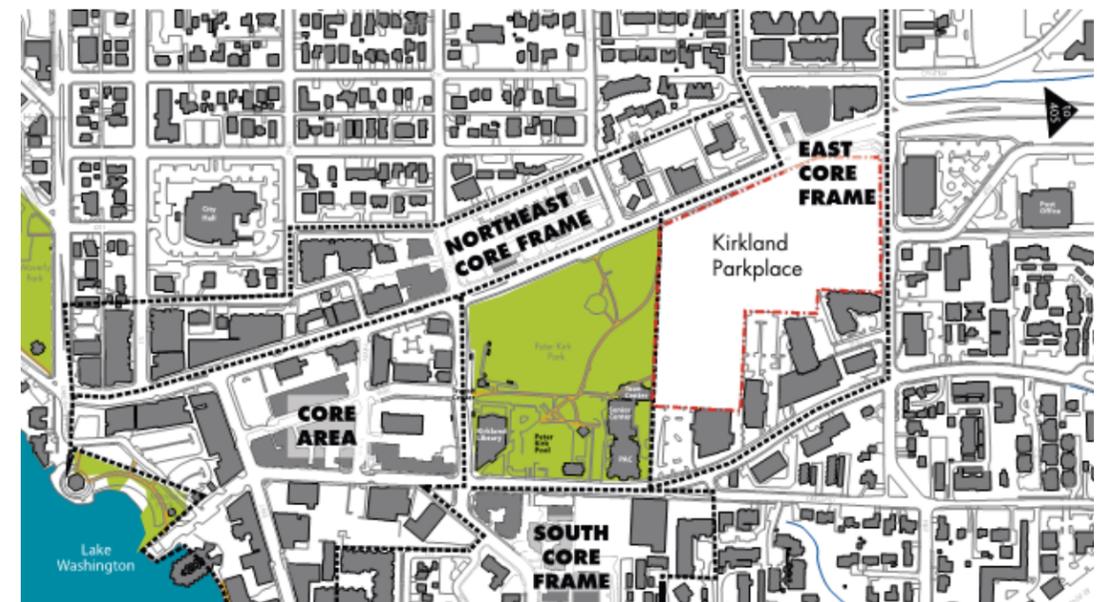
Kirkland Parkplace is both a home and a destination.



Kirkland Parkplace: Design District 5A, part of the East Core Frame in Kirkland's downtown area¹

1.1

This content of Comprehensive Plan policies and Master Plan responses has moved to Section 7: *Comprehensive Plan Design Direction*.



2008 Diagram: Site Context

3. Application

The Master Plan and Design Guidelines set forth in this document have been created to guide the development of Kirkland Parkplace to meet the intent of the vision for CBD-5A of the City of Kirkland. ~~Compliance with this Master Plan and Design Guidelines Document shall be required to~~ allows increased height and reduced setbacks in exchange for providing a mixed-use center and public amenities. These Standards and Guidelines are to be used in addition to the standard zoning regulations for CBD-5A. They are supplemental, not a substitution, to the City of Kirkland Municipal Code and its supporting documents.

4. Review Process: Determining Compliance

This document establishes performance criteria and provides recommendations for achieving specific design objectives. Compliance with the Master Plan, including general standards; general public amenity, and access locations; **organization of uses**; and street dimensional requirements shall be determined by administrative review (**planning official**). Compliance and consistency with the Design Guidelines shall be determined by the Design Review Board in a ~~Design Response conference~~. (Refer to DRB process **accordance with** KMC 142.35.9. In the DRB's review of the project, the Board shall respect the requirements and commitments established in this Master Plan.

4.1

6. Phasing

~~Depending on market conditions, this development shall~~ will be staged in ~~two~~ three major phases (~~1, 2-A, B, and C~~) with two minor stages in phase ~~A one~~ (A1 and A2).

Each independent phase will be designed and built to ensure that, at completion, there are no unsafe or unsightly temporary conditions and that pedestrian connections to and through the site are maintained and/or restored.

As a condition of design review approval for each phase, the applicant shall demonstrate how these conditions will be satisfied for that phase.

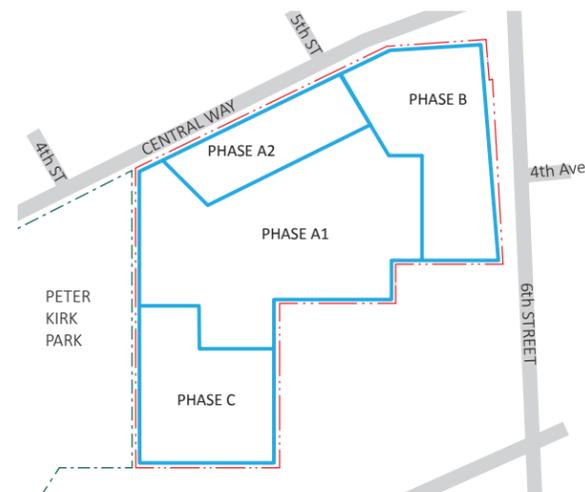
5. Modifications

A major modification to the Master Plan is any proposal that would result in a change that would substantially alter the Plan's proposed development such as: decrease in open space quantity, changes to locations of primary **and secondary internal access/pedestrian** streets, or changes in allowed use. Major modifications to the Master Plan shall require a staff review for consistency with the Comprehensive Plan and City Council approval. (Refer to KMC 3.30.040.)

A minor modification to the Master Plan, **reviewed by the Planning Director**, is any proposal that would result in a change that would not substantially alter the Plan's proposed development such as: facade treatments, street design variation, character/design detail of public spaces, or minor variations in design of sidewalks, pathways, lighting, and landscaping.

The Design Review Board may grant a design departure or minor variation **in the Design Guidelines** only if it finds that both of the following requirements are met:

- a. The variation is consistent with the intent of the guideline and results in superior design.
- b. The departure will not result in any substantial detrimental effect on nearby properties or the neighborhood.



Project phasing by location. Sequence of phasing to be determined.

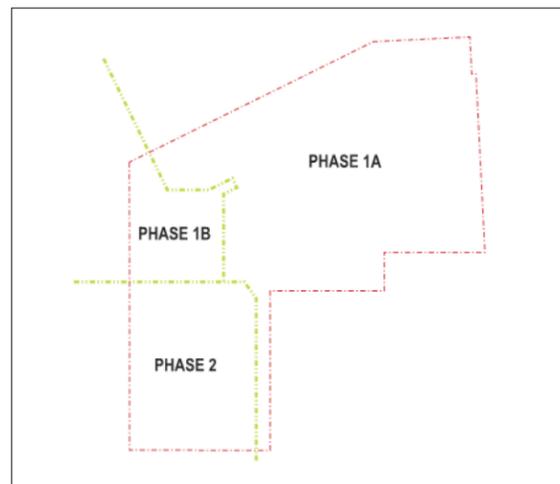
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NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

4.1 The content of the chart below, from the 2008 document, is covered in the text of Section 4. *Review Process: Determining Compliance.*

Section	Review Process
Development Standards <ul style="list-style-type: none"> • Program Requirements (Item 9) • Public Amenities and Access (Item 10) • Street Classifications (Item 11) 	Planning Official
Design Guidelines <ul style="list-style-type: none"> • All Districts (Item 12) • District Specific (Item 13) 	Design Review Board

2008 Diagram: Document Section and Review Process



2008 Diagram: Phasing

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NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

7.1 7. Comprehensive Plan Design Direction

The City of Kirkland's Comprehensive Plan, Section XV.D, includes several policies and guidelines directly related to the Parkplace site. Four relevant Comprehensive Plan directives and associated responses are included below:

A. CP Policy: Heights of up to eight stories are appropriate as an incentive to create a network of public spaces around which is organized a dynamic retail destination. (CP XV.D-13).

Response: Parkplace is an urban, open-air retail, restaurant, entertainment, office, ~~hotel and sports club~~ and residential complex. (See ~~page 7~~ Section 10 for standards regarding networks of open space, retail frontage, and pedestrian connections.)

B. CP Policy: Special attention to building design, size, and location should be provided at three key locations:

- at the intersection of Central Way and Sixth Street to define and enhance this important downtown gateway;
- along Central Way to respond to the context along the north side of street;
- and facing Peter Kirk Park to provide a transition in scale to downtown's central green space (CP XV.D-14).

Pedestrian Connections to adjoining streets, Peter Kirk Park, and adjoining developments should be incorporated to facilitate the integration of the district into the neighborhood (CP XV.D-13).

Response: Specific design guidelines have been defined to encourage unique environments and experiences in each of these three locations. The development standards define pedestrian connection requirements.

C. CP Policy: Because of the intensity of land use in 5A, the design of the buildings and site should incorporate aggressive sustainability measures, including low impact development measures, deconstruction, green buildings, and transportation demand management (CP XV.D-14).

Response: The compact development, pedestrian-friendly, mixed-use nature of the land use in CBD-5A is fundamentally sustainable. It provides a live-work balance in downtown Kirkland and provides access to goods and services people need in proximity to where they live. Combined with a commitment to sustainable strategies in the design of the development, Kirkland Parkplace will significantly contribute to lowering carbon emissions and energy use relative to a suburban model of development.

D. CP Policy: Residential development could be designed to integrate into both the office/retail character of the zone and the active urban nature of Peter Kirk Park (CP XV.D-14).

Response: The Development Standards provide for up to 30% of building floor area to be devoted to residential use. The proposed residential component will enhance Parkplace's public and retail experience and bring after-hours activity to the development. Residents will have access to a range of services and a direct connection to Peter Kirk Park - all within walking distance.

7.1 This section - "7. Comprehensive Plan Design Direction" - does not appear in the body of the 2008 document, though it is in the Table of Contents. The 2008 document included a discussion of relevant Comprehensive Plan design directives in Section 1, and this discussion has moved to the new Section 7.

7.2 This existing Comprehensive Plan statement related to residential development was not referenced in the 2008 Master Plan because residential development was not proposed at that time. It is included here, along with the Master Plan response, because the new Master Plan proposes a significant residential component on the site. See Section 9. Program Requirements.

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**NOTES ON CHANGES
& 2008 GRAPHICS TO BE REPLACED**

8. Design Intent

This Master Plan and Design Guidelines document was created using the identified **eight nine** Guiding Principles for the project which were derived from input from the City staff, Design Review Board, Planning Commission, various community groups, and the residents of Kirkland.

1. Emotional ownership by the community:
 - Incorporate the project into the story of Kirkland.
 - Enable meaningful community exchanges.
 - Inspire unique experiences and discoveries.
 - Promote the coalescence of Community, Culture, and Commerce.
 - Provide a 'transforming experience' vs. a 'transactional experience'.
 - Include neighborhood retail.
2. Site planning connections:
 - Include public spaces such as plazas.
 - Create clear vehicular access and parking.
 - Create strong emphasis on the streetscape.
 - Support active public spaces.
 - Provide clear and inviting public access.
 - **Provide connections to Peter Kirk Park.**
3. Create community gathering spaces:
 - Create easily accessible public spaces.
 - Develop spaces that vary in size and offer choices for all ages.
 - Provide safety and comfort.
 - Integrate into the social life of downtown Kirkland.
4. Enhance the pedestrian environment:
 - Promote walkability: network of internal and external pedestrian connections.
 - Create visual interest along the street.
 - Incorporate a rich variety of materials.
 - Provide and enhance pedestrian circulation and retail continuity.
5. Integrate motor vehicle access and parking
 - Minimize the visual presence of parked cars.
 - Allow parking to be utilized during nights/weekends for benefit of community and downtown.

6. A mix of uses = a mix of building types:
 - Create a variety of building types, scales, and materials.
 - Express a three-dimensional quality to the public spaces.
7. Appropriate massing and scale:
 - Create pedestrian spaces with access to sun.
 - Address surrounding edges.
 - Consider scale, massing, and detail of individual buildings.
 - Express human-scale, detailed street level building facades.
8. Sustainability:
 - Establish macro-scale/site sustainable strategies.
 - Pursue building-specific sustainable strategies.
 - Encourage tenant-specific sustainable strategies.
9. **Mixed-use development:**
 - **Provide a residential component to the project that will support the viability of a 24-hour development and complement the other uses on the site.**



Children's play area at Peter Kirk Park²

MASTER PLAN: DEVELOPMENT STANDARDS

9. Program Requirements

The following requirements and ratios are established to quantify use types at the completion of the project, and are not a requirement for any single phase.

A. PEDESTRIAN SPACE

The development will include a variety of public open spaces that vary in size and character. A minimum of 10%, or 50,000 square feet, of the site shall be activated pedestrian-oriented space, in the form of courtyards, plazas, winter atrium, etc. See diagram (Section 10.D) for approximate locations and dimensional requirements of specific spaces. Definition of appropriate design treatments are found in the district-specific design guidelines (Section 13).

B. ARTS COMMITMENT

In an effort to encourage integrated art into the project, Parkplace is working in collaboration with representatives from the cultural council and local art community and will identify and create opportunities to integrate art into the project.

C. GREEN BUILDING COMMITMENT

Section V. Natural Environment of the Comprehensive Plan outlines broad goals and policies related to environmental sustainability. Section XV.D of the Comprehensive Plan and Guiding Principle #8 (see Section 8 of this document) describe goals specific to the Parkplace site.

1. In response to these goals and policies, the following requirements will apply to the Kirkland Parkplace project:

- a. All new office buildings will be designed achieve a LEED CS Gold threshold. A USGBC Pre-Certification Application showing points meeting LEED CS Gold will be included with permit submittals to show which points will be pursued.
- b. The ~~hotel~~ multi-family residential building(s) will be designed to a LEED for Homes Multifamily Mid-Rise Silver ~~CS Certified or LEED-NC Certified~~ threshold; or to meet Built Green 4 Star certification ~~the sustainability program of the hotel operator.~~

- c. The applicant shall encourage all potential tenants for Kirkland Parkplace to pursue LEED-CI. To accomplish this, the applicant will create and distribute to tenants a set of Tenant Design Guidelines to show strategies tenants can use to achieve LEED-CI certification. These Tenant Design Guidelines will be made available to the City of Kirkland to inform their ongoing sustainability programs.

- d. At the end of tenant build-outs ~~on~~ of the office space, the applicant ~~will~~shall prepare an executive summary for the City of Kirkland, outlining what sustainability measures were incorporated in the tenant build-outs (unless otherwise restricted by tenant confidentiality).

- e. In addition, the applicant ~~will~~shall strive to make design choices in its Core and Shell buildings that are conducive to the achievement of LEED-CI by tenants.

2. In the interest of promoting a holistic sustainability approach, the applicant ~~will~~shall strive to integrate site-specific strategies identified as focus areas, such as:

- a. Energy efficiency strategies, like centralized cooling options and heat recovery.
- b. Low Impact Development (LID) strategies like stormwater planters, vegetated roofs, and bioswales.
- c. Materials and resource strategies like recycled materials, regional materials, and FSC certified wood.

D. COMMUNITY-SERVING RETAIL AND SERVICES

Include neighborhood-serving retail and services. ~~such as~~ Possible examples include: grocery, childcare, bookstore, drugstore, dry cleaner, movie theater, barbershop, shoe repair, etc.

E. PARKING

To guide the transformation described in the Comprehensive Plan from "an auto-oriented center surrounded by surface parking into a pedestrian-oriented center integrated into the community" (CP XV.D-13), the majority of parking for the development shall be placed underground. Surface parking will be provided along selected internal streets and at other selected surface parking locations to support retail uses.

TOTAL SITE AREA = 501,000 sf = 100%

SITE AREA BREAKDOWN

Building Footprint 38 - 43%	Open Space* 30 - 35%	Vehicle Areas 22-27%
---------------------------------------	--------------------------------	--------------------------------

*Open Space refers to all at-grade site area other than building footprints, streets, or parking areas. The 50,000 square foot requirement applies specifically to *Pedestrian Space*, outlined below, and in Section 10D: *Pedestrian Space*.

OPEN SPACE BREAKDOWN

Sidewalks 20-26% of Site	PEDESTRIAN SPACE Plazas/Courtyards/Gardens ≥ 50,000 sf or 10% of site	Public Roof Terrace 10,000 sf	Private Roof Terrace 10,000 sf
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BUILDING USE BREAKDOWN - Approximate 1,140,000 GROSS SF TOTAL

Commercial Office 600,000 - 615,000 sf	Retail / Fitness / Entertainment 210,000 - 225,000 sf	Residential 250-300 units 280,000 - 300,000 sf
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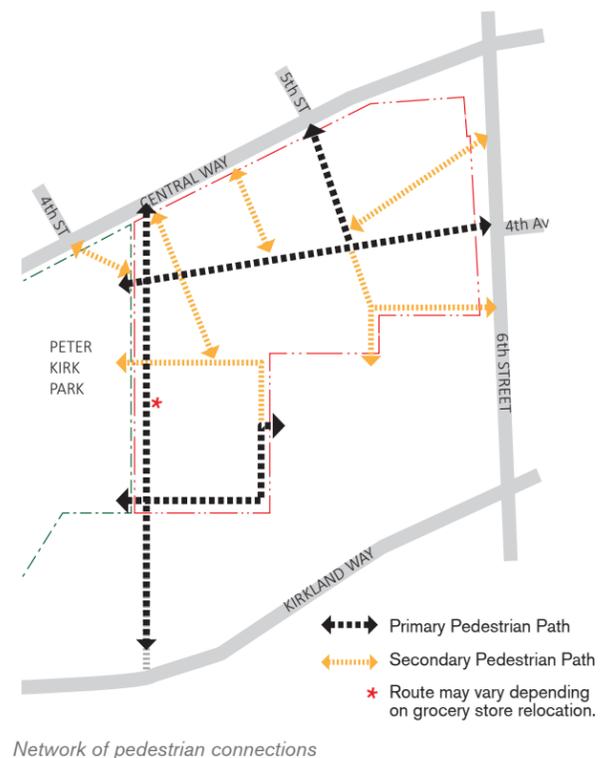
10. Public Amenities, Access, and Organization of Uses

A. PEDESTRIAN CONNECTIONS

Intent: Create a network of identifiable linkages into and through the project site for pedestrians.

The diagram at right shows approximate pedestrian connections. Darker lines indicate primary connections required designated by the Comprehensive Plan. Lighter lines show secondary connections linking existing proposed streets as well as Peter Kirk Park. These connections are for public use.

The applicant shall work with the City to define appropriate wayfinding strategies between the development and the Cross Kirkland Corridor.



SITE AREA 501,000 SQFT (100%)

SITE AREA BREAKDOWN

BUILDING FOOT PRINT 245 K (49%)	OPEN SPACE 160 K (32%)	VEHICULAR AREAS 96 K (19%)
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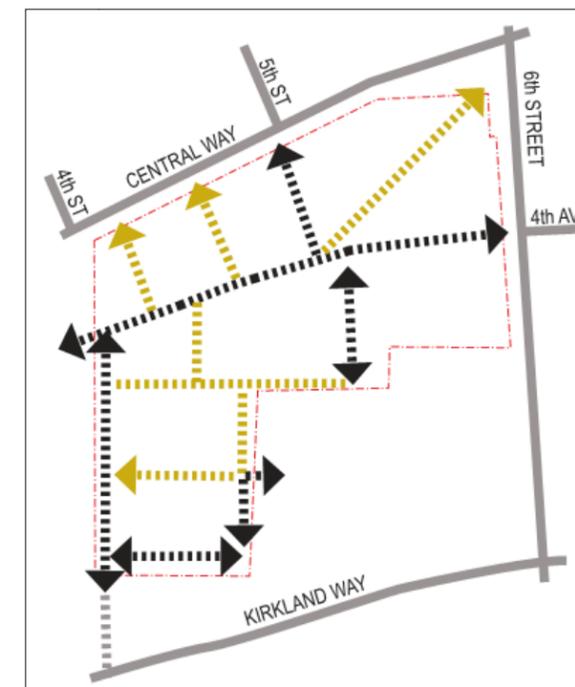
OPEN SPACE BREAKDOWN

SIDEWALKS 109 K (22% OF TOTAL SITE AREA)	PEDESTRIAN SPACE PLAZAS 27 K (5%)	COURTYARD 24 K (5%)	PUBLIC ROOF TERRACE 10 K	PRIVATE ROOF TERRACE 10 K
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BUILDING GROSS SQUARE FOOTAGE BREAKDOWN

OFFICE OFFICE SPACE 1.2 MIL SQFT 68%	RETAIL	HOTEL
	MIX USE 550K SQFT 32%	

2008 Diagram: Area Breakdown



2008 Diagram: Pedestrian Connections

B. RETAIL/RESTAURANT FRONTAGE

Intent: Encourage and contribute to the liveliness and activation of pedestrian-oriented streets and spaces by providing retail and activating uses at ground level.

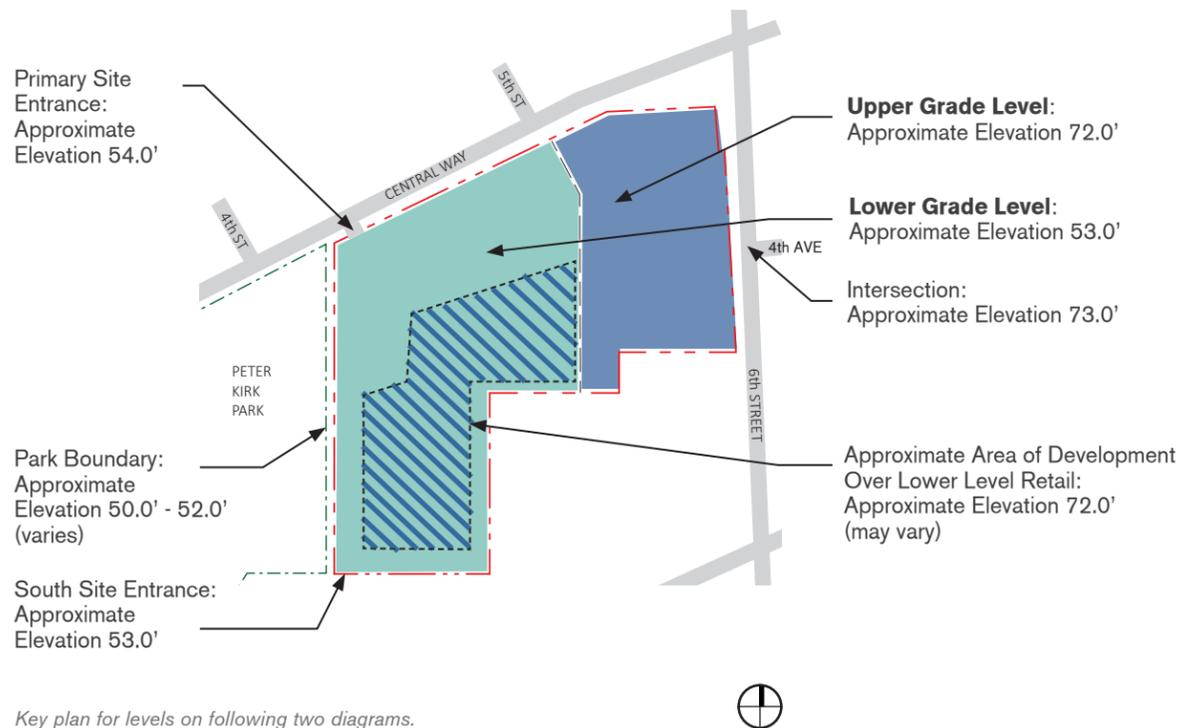
Predominant retail and other pedestrian-encouraging uses, including shops, restaurants, grocery, and a movie theater are required encouraged along pedestrian-oriented streets and public spaces. Additional activating uses are encouraged on the ground level throughout the development where feasible.

C. ORGANIZATION OF USES

Intent: Locate building and other uses to support the development goals of the project, including: ground floor retail, upper floor office space, residential space, and public gathering spaces between buildings.

The following diagrams describe the general approximate locations of various building use types, pedestrian connections, parking, and public gathering spaces.

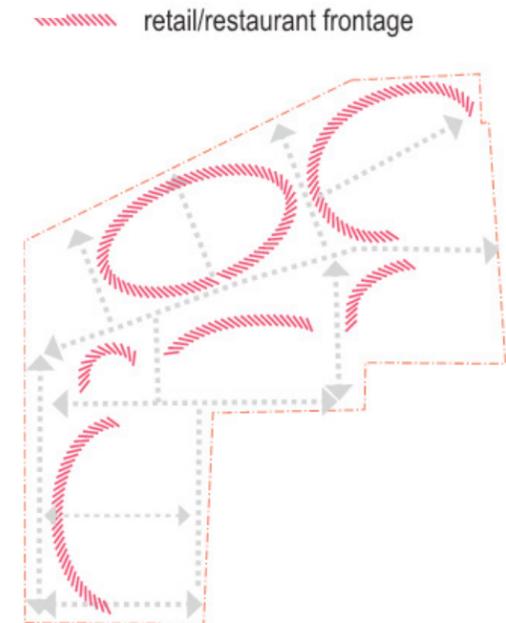
The key plan below illustrates the two grade levels for the site: *Upper Grade Level* and *Lower Grade Level*. The Upper Grade Level relates to the existing street grades at the intersection of 6th Street and 4th Avenue. The Lower Grade Level relates to Peter Kirk Park and the grades at the primary site entrance on Central Way.



Key plan for levels on following two diagrams.

**NOTES ON CHANGES
& 2008 GRAPHICS TO BE REPLACED**

The Retail/Restaurant Frontage diagram is removed. The content of this 2008 diagram is folded into the Organization of Uses diagrams in Section 10.C. Retail frontage is no longer included at the northeast corner of the site. Other activating uses are encouraged there instead.



2008 Diagram: Retail/Restaurant Frontage

Section 10.C Organization of Uses is new to the 2014 document.

**C. ORGANIZATION OF USES
(continued)**

LOWER GRADE LEVEL

BUILDINGS

- A** Retail with entries accessed from internal street;
Office above
- B** Retail and Grocery with entries accessed from internal
street and/or open space;
Residential above
- C** Retail and/or Entertainment;
Office above

SITE

- D** Below-Grade Parking
- E** Retail Surface Parking
- F** Pedestrian Space:
Plaza/Courtyard/Garden
- Vehicular and
Pedestrian Circulation
- Pedestrian-Only
Circulation
- ▶ Vehicle Site Access
- ▶ Pedestrian Site Access;
Locations to be Determined
- ▶ Provide visibility into
retail or other activating
uses at these locations
- Retail
- Outdoor Amenity
- Parking



**NOTES ON CHANGES
& 2008 GRAPHICS TO BE REPLACED**

Section 10.C Organization of Uses is new to the 2014 document.

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**NOTES ON CHANGES
& 2008 GRAPHICS TO BE REPLACED**

Section 10.C Organization of Uses is new to the 2014 document.

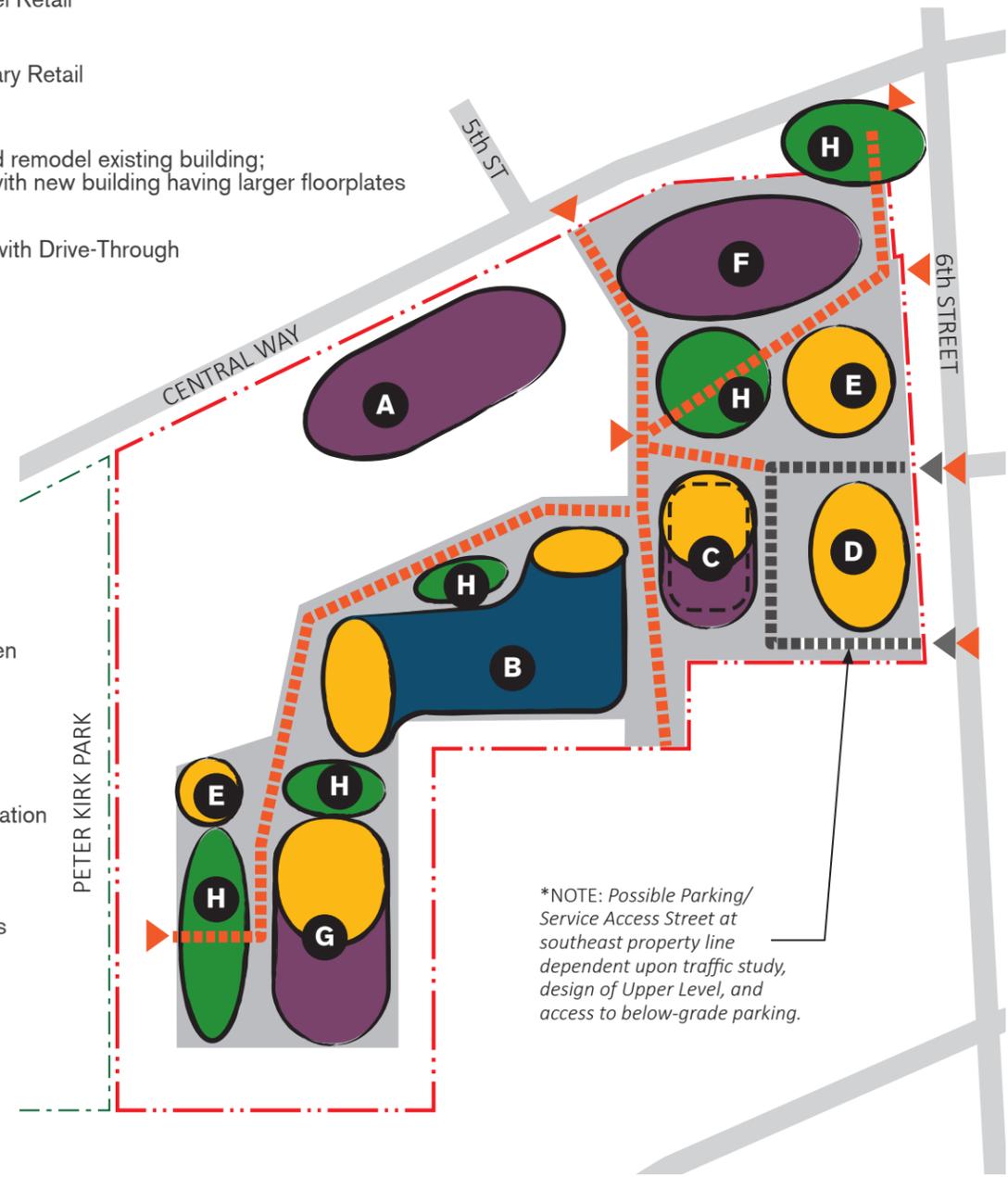
**C. ORGANIZATION OF USES
(continued)
UPPER GRADE LEVEL**

BUILDINGS

- A** Office over Lower Level Retail
- B** Residential with Ancillary Retail
- C** Office with Retail;
Options: 1) Retain and remodel existing building;
2) Replace with new building having larger floorplates
- D** Retail: Possible Bank with Drive-Through
- E** Retail
- F** Office
- G** Office with Retail

SITE

- H** Pedestrian Space:
Plaza/Courtyard/Garden
and/or
Roof Terrace
- Vehicular and
Pedestrian Circulation
- Pedestrian-Only Circulation
- ▶ Vehicle Site Access
- ▶ Pedestrian Site Access
- Retail
- Outdoor Amenity
- Office
- Residential



**NOTE: Possible Parking/
Service Access Street at
southeast property line
dependent upon traffic study,
design of Upper Level, and
access to below-grade parking.*

DRAFT
APPENDIX I: ROAD MAP TO CHANGES

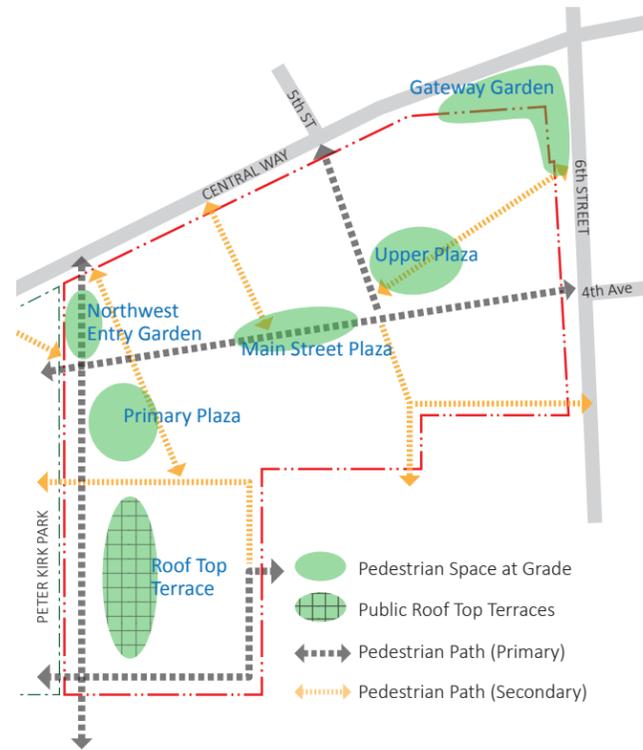
D. PEDESTRIAN SPACE

Intent: Provide a functional and diverse pedestrian environment by creating a variety of usable pedestrian open spaces.

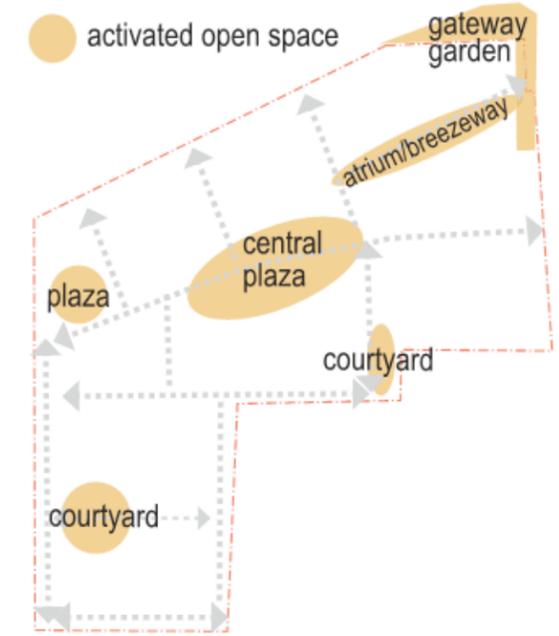
The following types of public/pedestrian space are to be provided at a minimum of 10% of the total lot area, or 50,000 square feet. Locations are approximate and not limited to those shown on the diagram at right below.

- a. Central Primary plaza:** shall have a minimum area of 12+5,000 square feet with a minimum average width of 70 feet.
- b. c. Atrium/breezeway- Main Street plaza:** a linear sequence of pedestrian spaces along Main Street retail shall have locations with a minimum 35-foot width-wide separation between office floor plates.
- c. Upper Plaza:** shall include a combination of landscaping and hardscaping with a minimum area of 10,000 square feet.
- d. Northwest Entry Garden:** shall be predominantly landscaped and an extension of Peter Kirk Park.
- e. b. Smaller courtyard/plazas:** shall have a minimum area of 2,500 square feet each. (not illustrated in diagram at right)
- f. d- Roof top terraces:** shall provide a minimum of 10,000 square feet total of publicly accessible rooftop terraces in one or more locations.

See district specific guidelines for design parameters of public space (ex. central plazas, Section 13.D).



Distribution of pedestrian spaces: along paths, between buildings, and on rooftop terraces. Locations are illustrative and subject to change.



2008 Diagram: Pedestrian Space

11. Street Classification

Intent: Create a street and sidewalk network that responds to the existing Kirkland grid pattern, creates a pedestrian-oriented environment, and allows for direct interaction with Peter Kirk Park.

The following street classifications and diagrams represent the various types of streets and approximate locations anticipated in the project. Final location and classification of streets may be adjusted in the final design to include such design techniques as: tight turning radii to calm traffic, curb bulb outs, textured crossings, etc. Access shall be in compliance with city codes and polices for public improvements and emergency access.

Street classifications are meant to be typical sections of the roadway. Slight variations may be necessary to accommodate driveways, street furniture, structural constraints, etc.

Planting adjacent to parking or drive lane may consist of tree wells level with sidewalk or planting strips which are flush with sidewalk or raised above sidewalk. Where tree wells occur, provide minimum 12'-0" total sidewalk width including tree wells, with minimum sidewalk width of 8'-0" and tree grate width of 4'-0" (except as noted on street sections).

Where continuous planting strips are provided in lieu of street tree wells, provide minimum 10'-0" sidewalk and 4'-0" minimum planting strip (unless noted otherwise).

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ADJACENT PUBLIC STREET IMPROVEMENTS

- 1 Central Way
- 2 6th Street

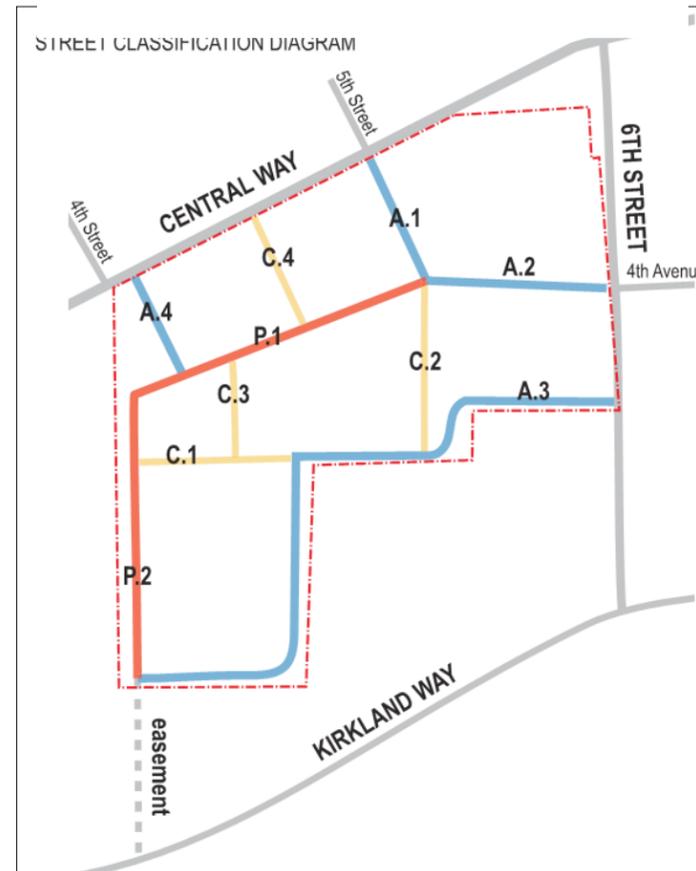
PRIMARY INTERNAL STREETS

- 3 Park Promenade
- 4 Main Street

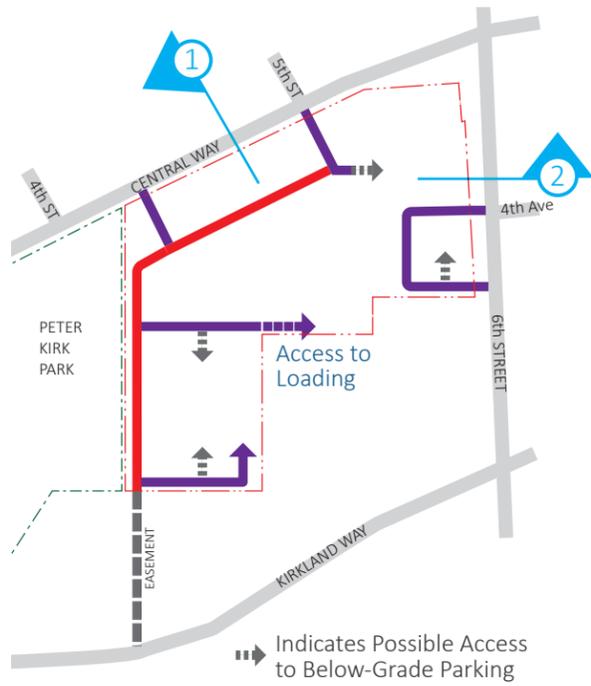
SECONDARY INTERNAL STREETS

- 5 Access Street at Central Way near 4th Street
- 6 Access at Central Way near 5th Street
- 7 Access at 6th Street
- 8 Upper Level Internal Street
- 9 Possible Parking/Service Access at 6th Street
(Dependent upon traffic study, design of Upper Level, and access to below-grade parking)
- 10 Access Street at Southern Property Line
- 11 Parking/Service Access

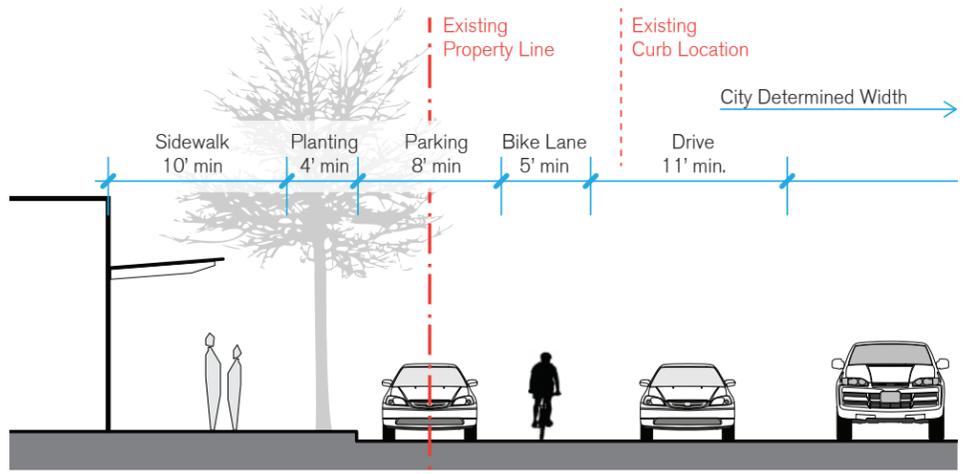
**NOTES ON CHANGES
& 2008 GRAPHICS TO BE REPLACED**



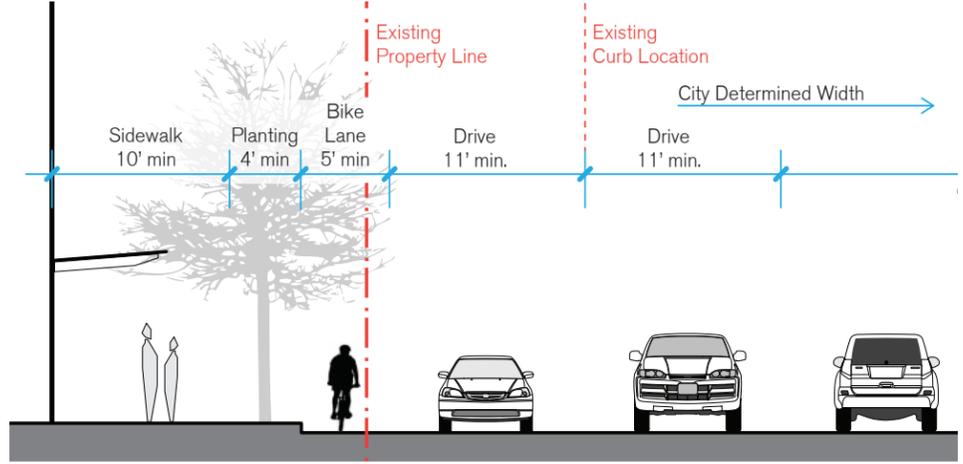
ADJACENT PUBLIC STREET IMPROVEMENTS



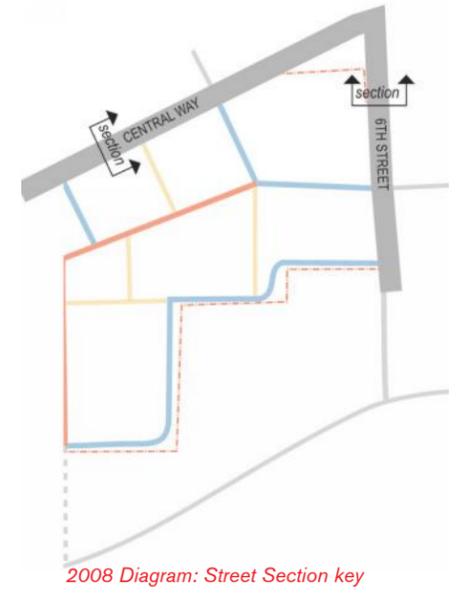
1 Central Way Frontage Section (typical)



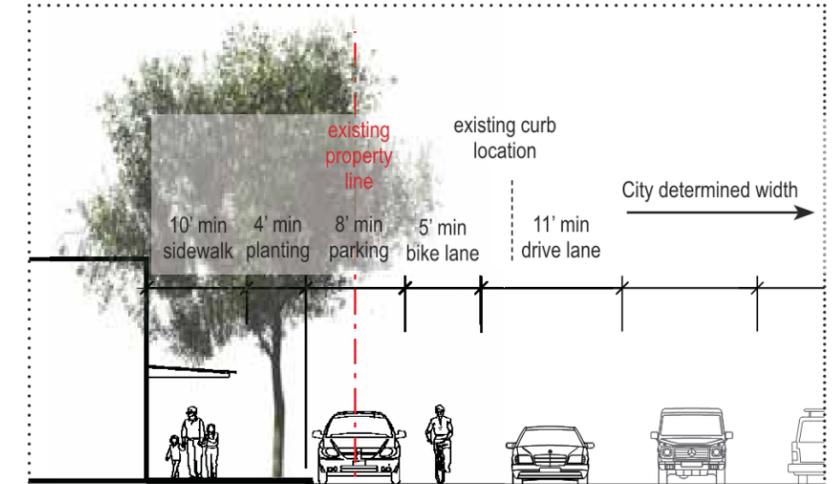
2 6th Street Frontage Section (typical north of 4th Avenue)
(Confirm with City of Kirkland)



**NOTES ON CHANGES
& 2008 GRAPHICS TO BE REPLACED**



2008 Diagrams: Adjacent Public Street Improvement Street Sections



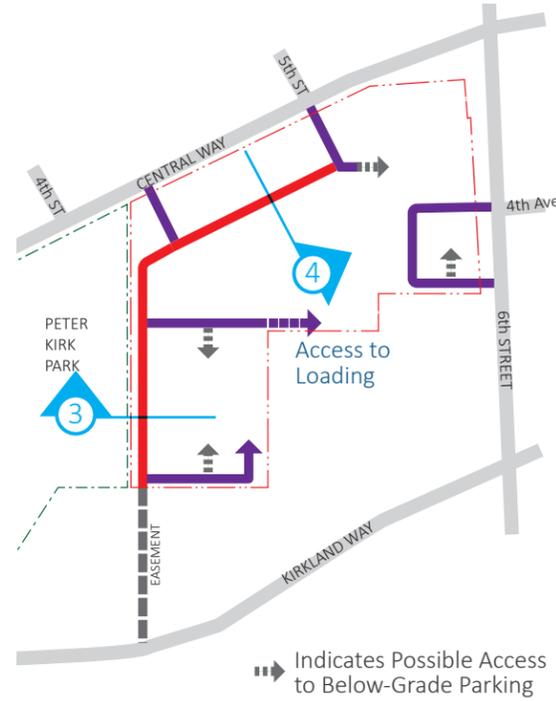
CENTRAL WAY FRONTAGE TYPICAL SECTION



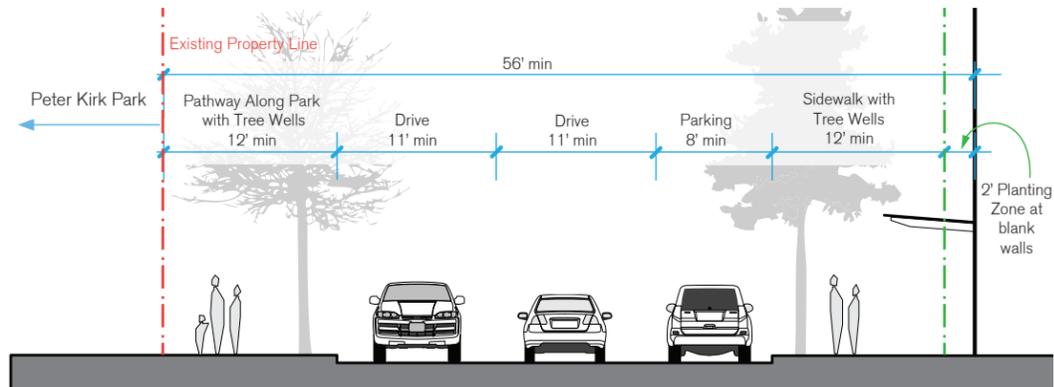
6TH STREET TYPICAL SECTION

DRAFT APPENDIX I: ROAD MAP TO CHANGES

PRIMARY INTERNAL STREETS

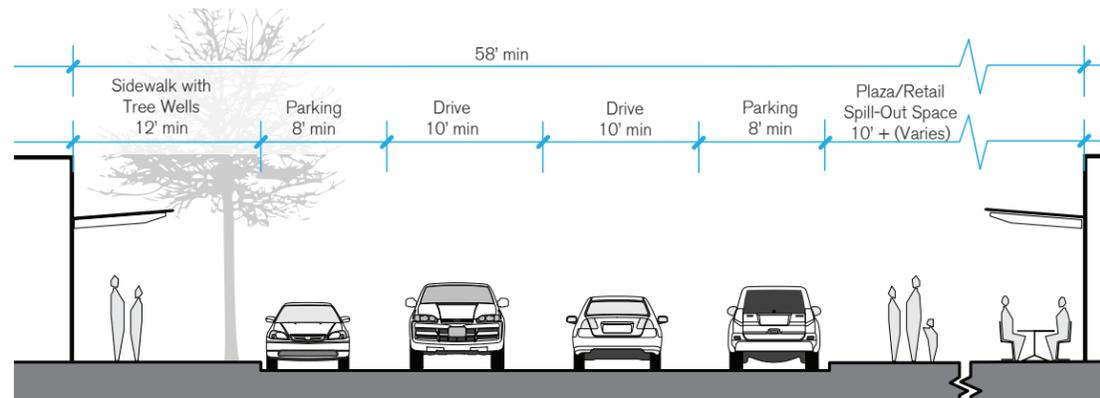


3 Park Promenade Section (typical)

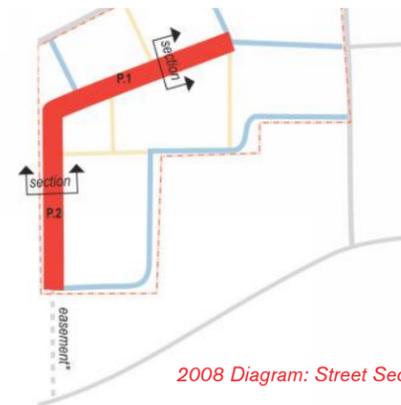


The existing easement to the south shall include a pedestrian sidewalk connecting the Park Promenade with Kirkland Way.

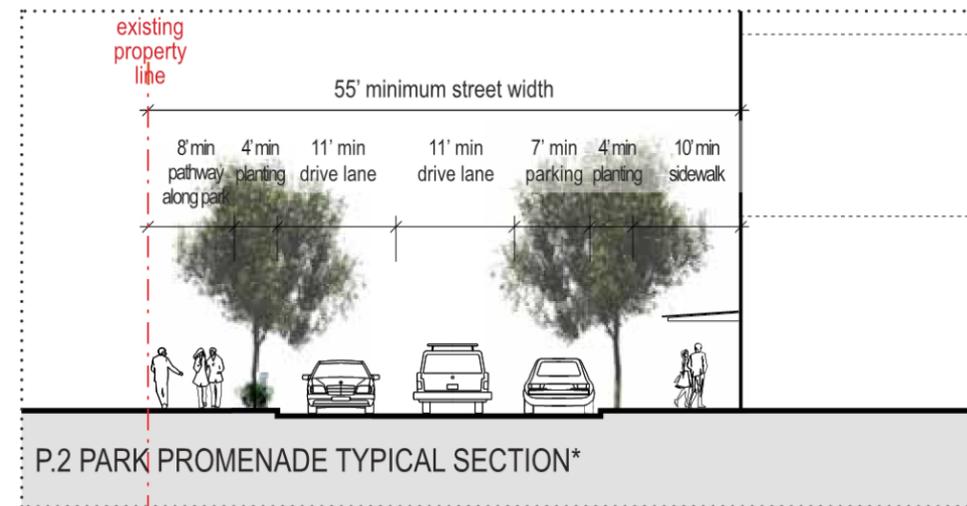
4 Main Street Section (typical)



NOTES ON CHANGES
& 2008 GRAPHICS TO BE REPLACED

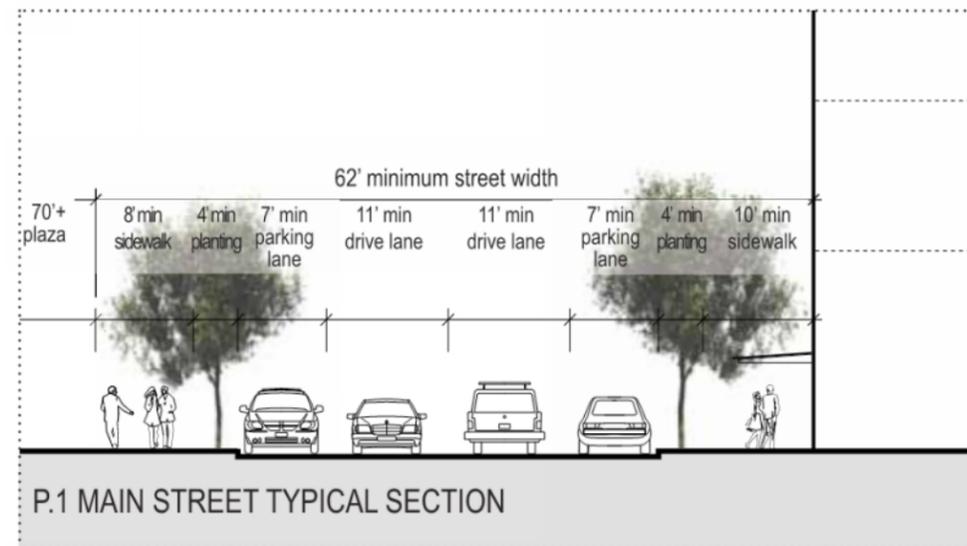


2008 Diagram: Street Section key



P.2 PARK PROMENADE TYPICAL SECTION*

* The existing easement to the south shall include a pedestrian sidewalk connecting the Park Promenade with Kirkland Way.

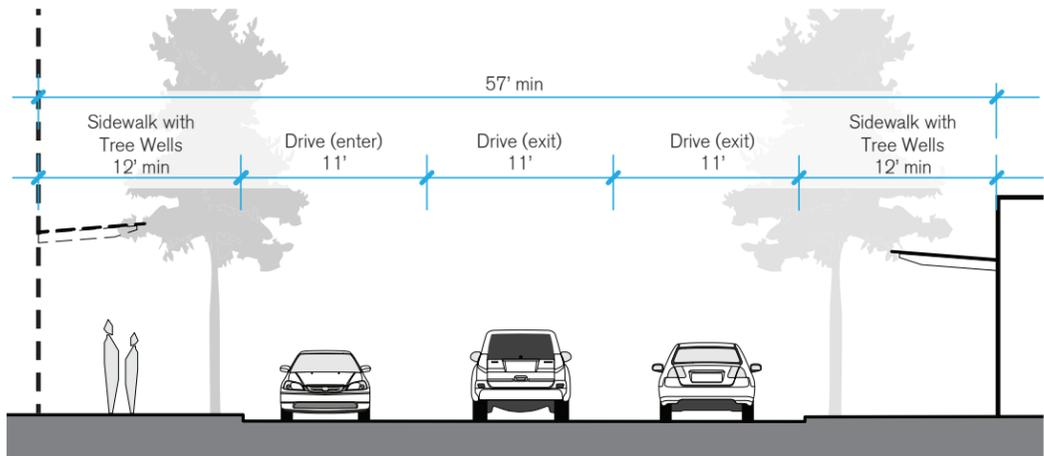


P.1 MAIN STREET TYPICAL SECTION

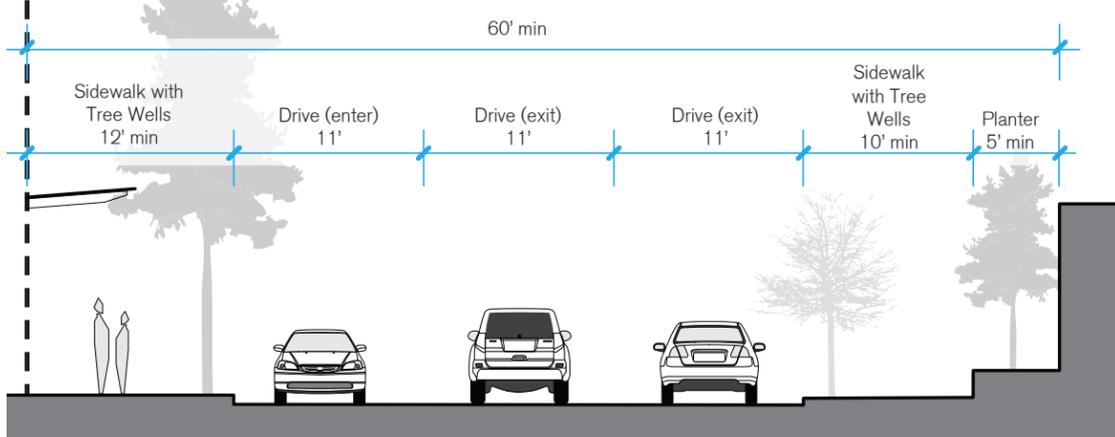
2008 Diagrams: Pedestrian Street Sections

SECONDARY INTERNAL STREETS

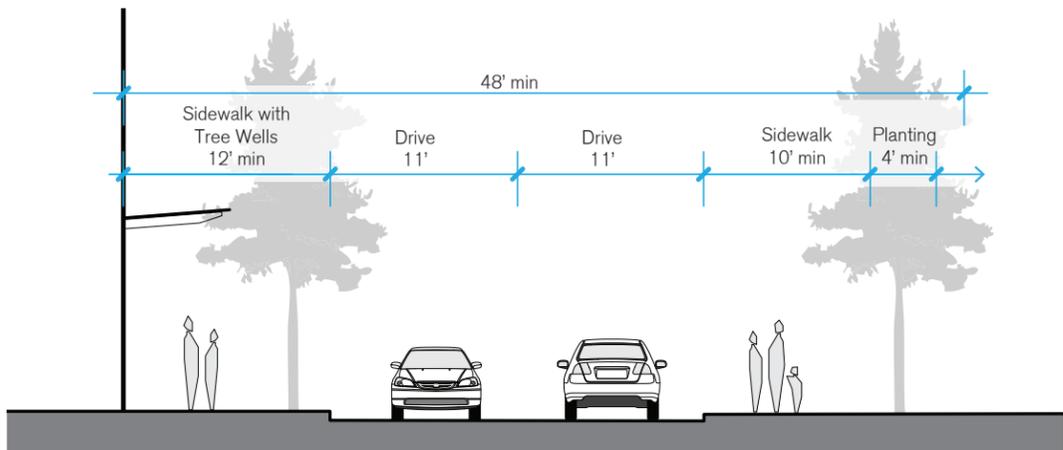
5 7 Access Streets at Central Way, 6th Street (typical)



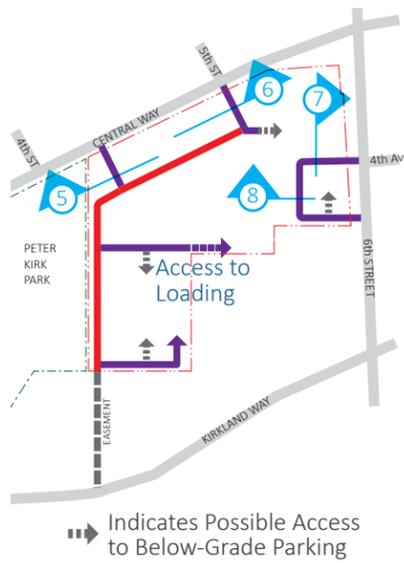
6 Access Street at Central Way and 5th Street (typical)



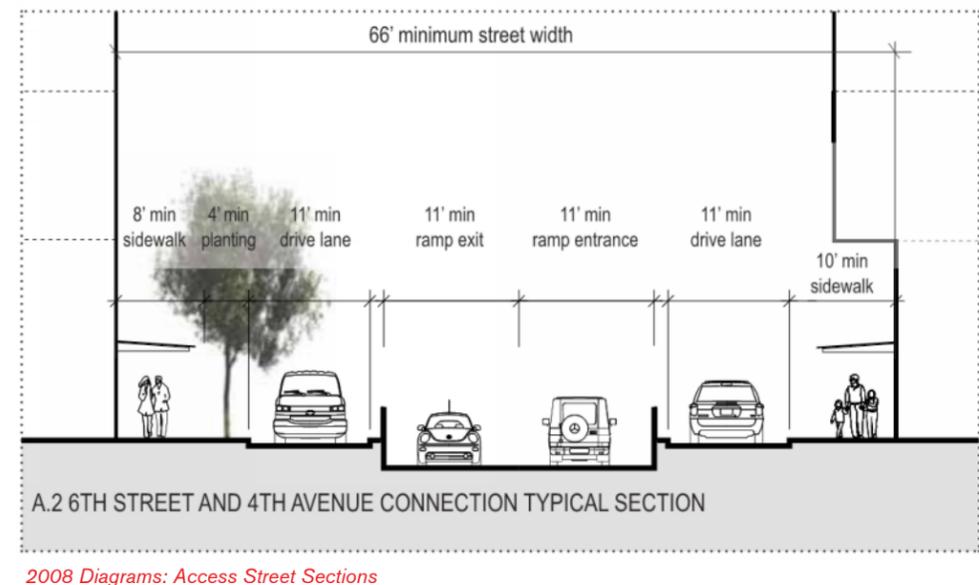
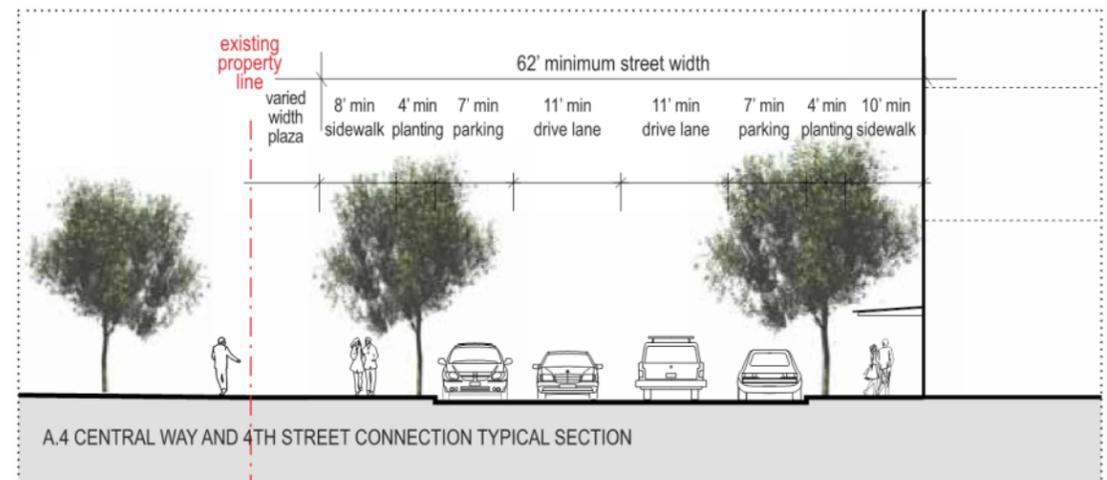
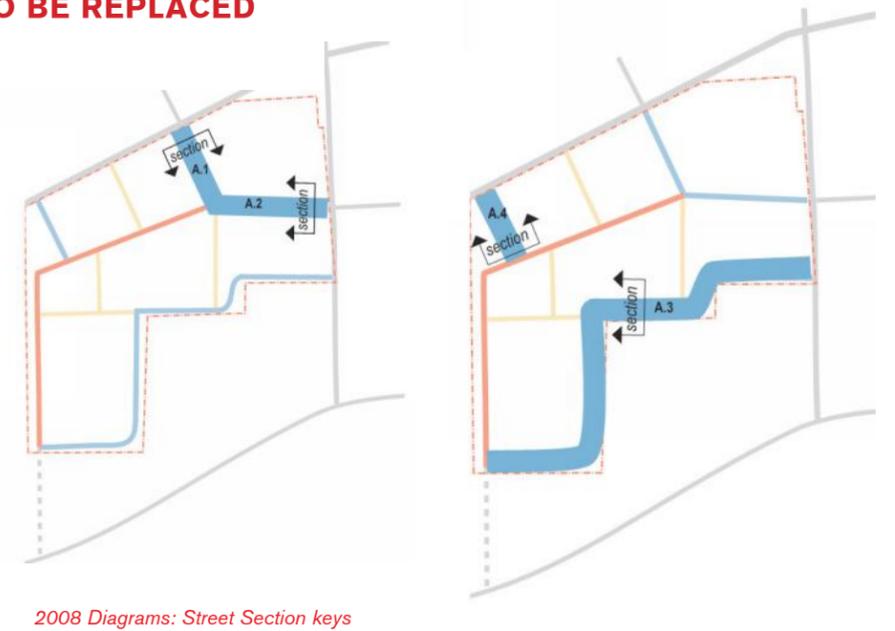
8 Upper Level Internal Street (typical)



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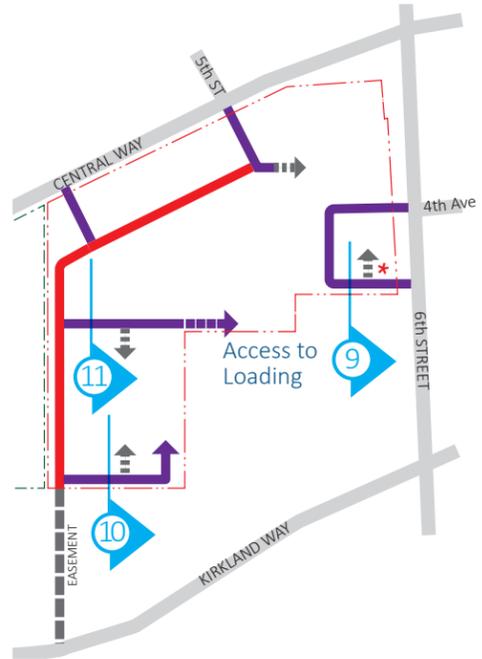
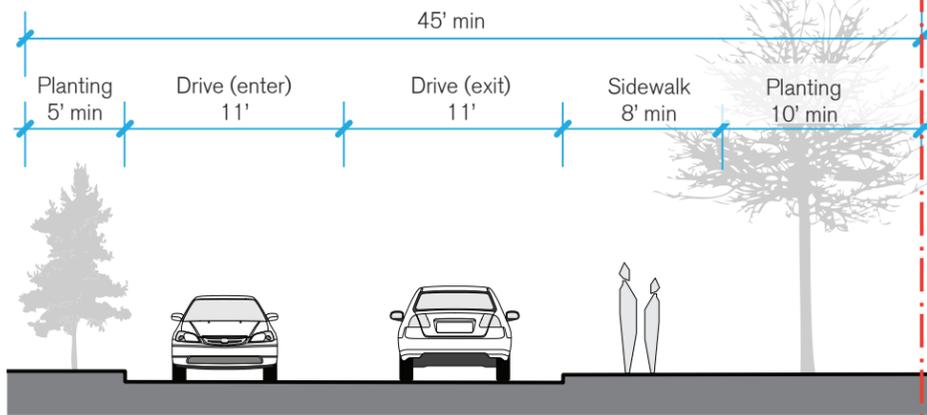


2008 Diagrams: Access Street Sections

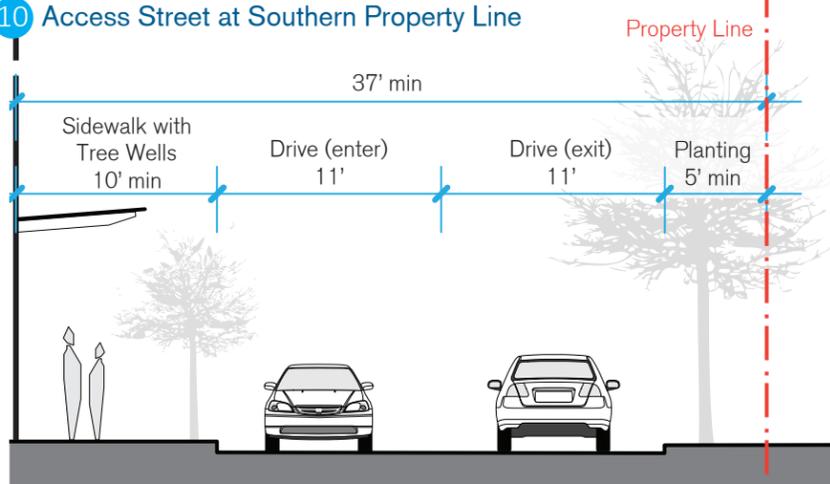
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SECONDARY INTERNAL STREETS

**9 Possible Parking/Service Access at 6th Street
(Confirm with City of Kirkland)**

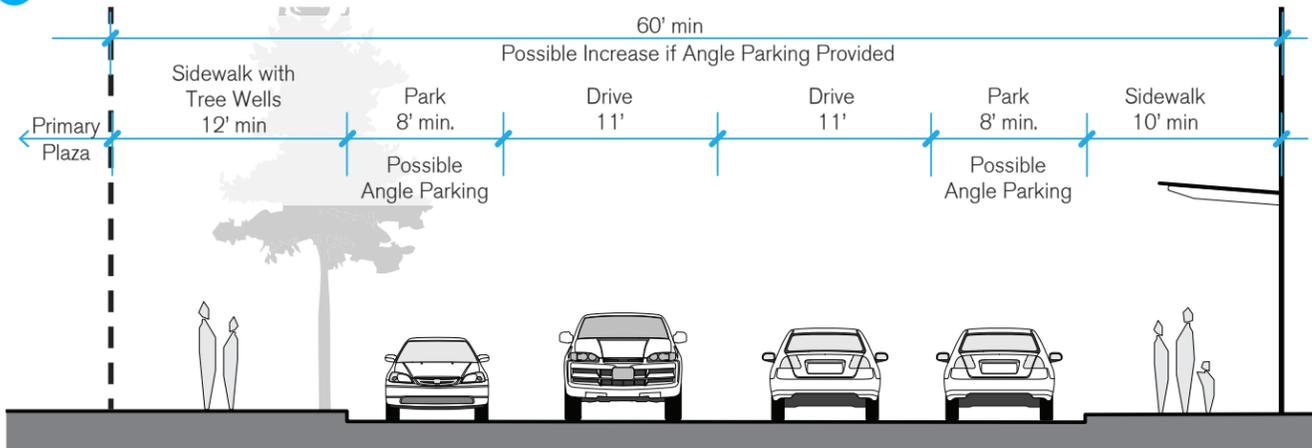


10 Access Street at Southern Property Line

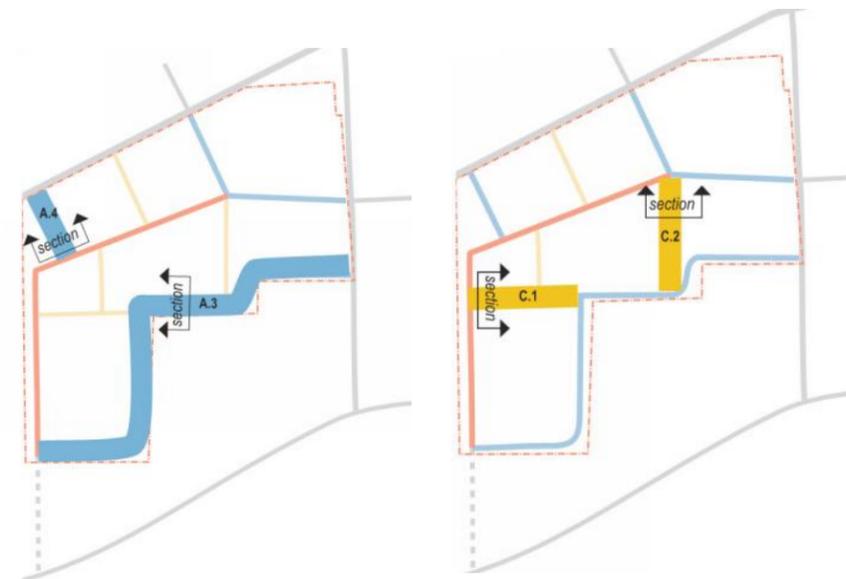


Indicates Possible Access to Below-Grade Parking
* Access Point T.B.D. per Traffic Study

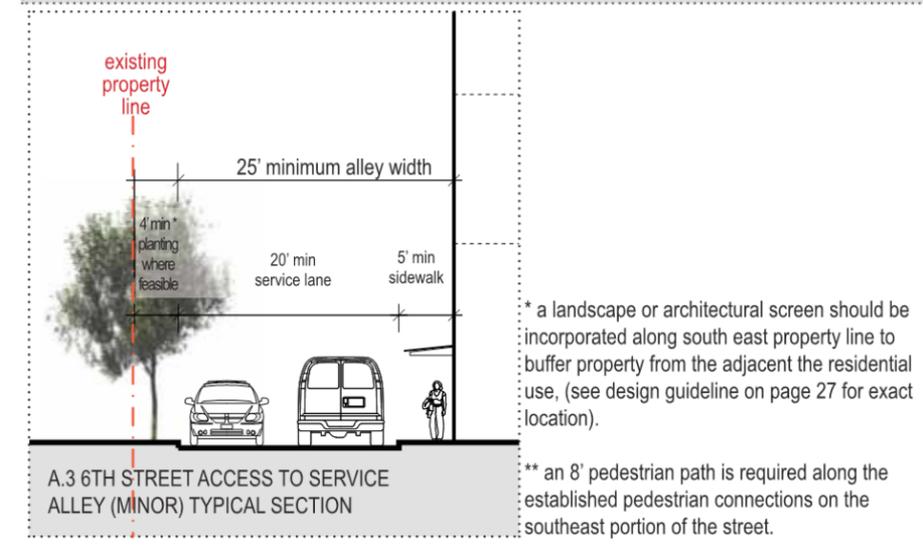
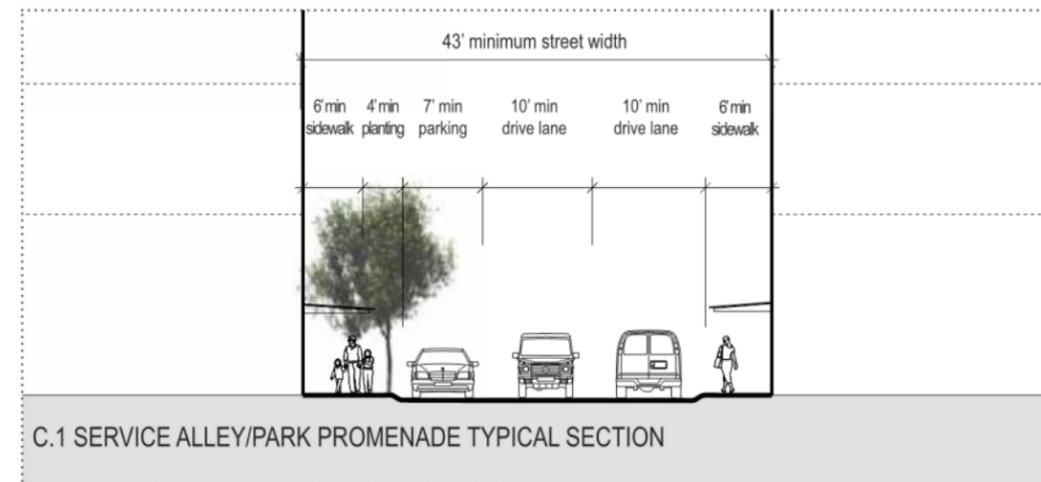
11 Parking/Service Access (typical where parking occurs)



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2008 Diagrams: Street Section keys

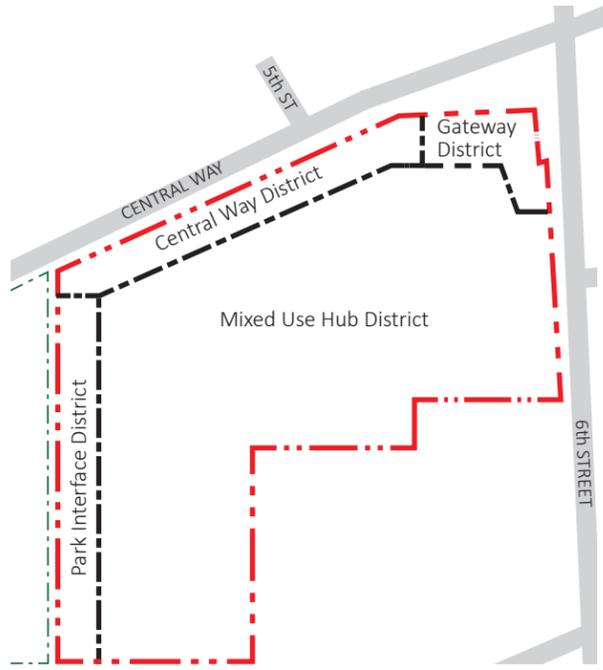


2008 Diagrams: Access/Service Street Sections

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DESIGN GUIDELINES

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Key Plan: on-site district locations

The Guidelines in Section 12 apply to all districts. Section 13 identifies Guidelines that are district-specific and respond to key locations defined in the City's Comprehensive Plan as requiring special attention. These design districts are defined in the diagram at left.

12. Design Guidelines: All Districts

Overall Intent: Create a rich pedestrian-oriented environment and successful mixed-use center.

SITE PLANNING

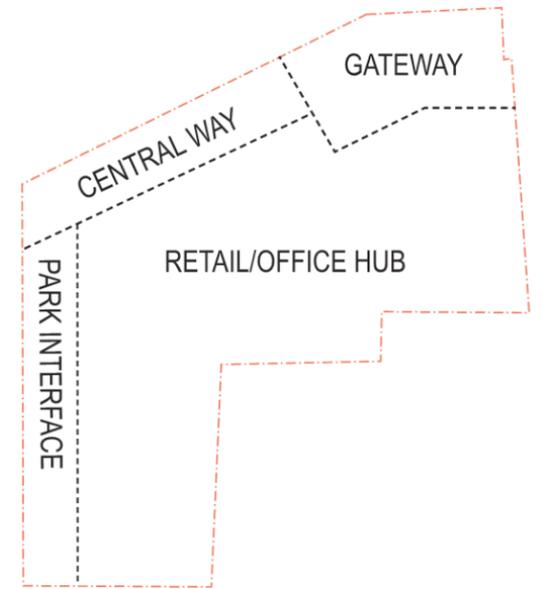
1. STREETScape

Intent: Maintain a continuous and safe streetscape with a pedestrian-friendly character.

- a. Sidewalks should maintain at least an 6-8 ft clear zone for pedestrian travel (except as noted in street sections).
- b. All streets should contribute to the physical safety and comfort of pedestrians. Provide the following where feasible to help define the sidewalk space:
 - on-street parking (see street classifications)
 - a well-defined amenity zone set to the curb for plantings, street trees, benches, trash receptacles, signs etc. (Minor deviations for street trees and major planting spaces may be necessary in some spaces due to structural constraints.) 12.1
 - wide enough sidewalk space to accommodate outdoor seating where restaurants are anticipated
- c. Use design elements such as separate storefronts, pedestrian-oriented signs, exterior light fixtures, awnings and overhangs to add interest and give a human dimension to street-level building facades.
- d. In general, buildings with active ground floor uses should be set as close as possible to sidewalk to establish active, lively uses. Maintain a continuous street wall, limiting gaps to those necessary to accommodate vehicular and pedestrian access.
- e. Encourage recessed main building and/or shop entrances consistent with a traditional "main street" design that is inviting and promotes streetscape continuity.



Pedestrian-friendly character: on-street parking; amenity zone with street trees, signs, light fixtures; wide sidewalk to accommodate outdoor seating.



2008 Diagram: District key plan

12.1 This note ("Minor Deviations...due to structural constraints") is at the end of the Streetscape section in the 2008 version.

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- f. The corners of buildings located at street intersections may recess to promote visibility and allow for a collection of people.
- g. Allow larger buildings to recess from the sidewalk edge to allow for entry forecourts, provided street continuity is not interrupted along the majority of the block.

*Minor Deviations... [moved to 12.1.b.]

2. PUBLIC SPACES: PLAZAS, COURTYARDS, AND SEATING AREAS GARDENS

Intent: Provide a friendly pedestrian environment by creating a variety of usable and interesting public and semi-public open spaces within private development.

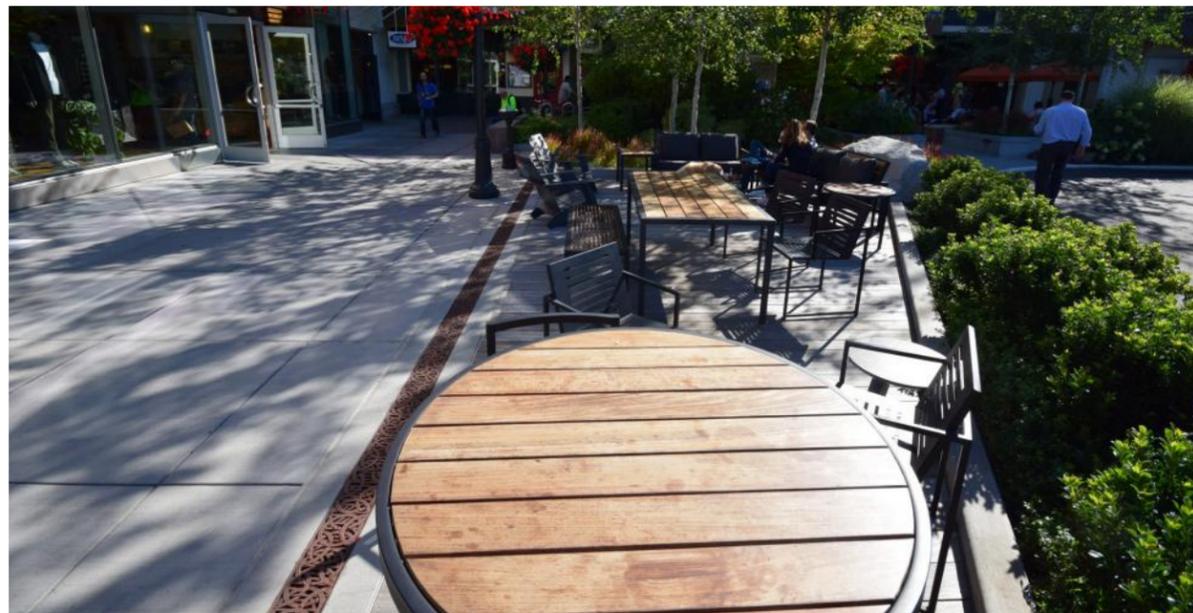
- a. Make plazas and courtyards comfortable for human activity and social interaction – standing, sitting, talking, eating, etc.
- b. Define and contain outdoor spaces through a combination of building and landscape. Oversized spaces that lack containment are discouraged.
- c. Establish pedestrian pathways that link public spaces to other public spaces and streets. These should be clearly identifiable for easy wayfinding.

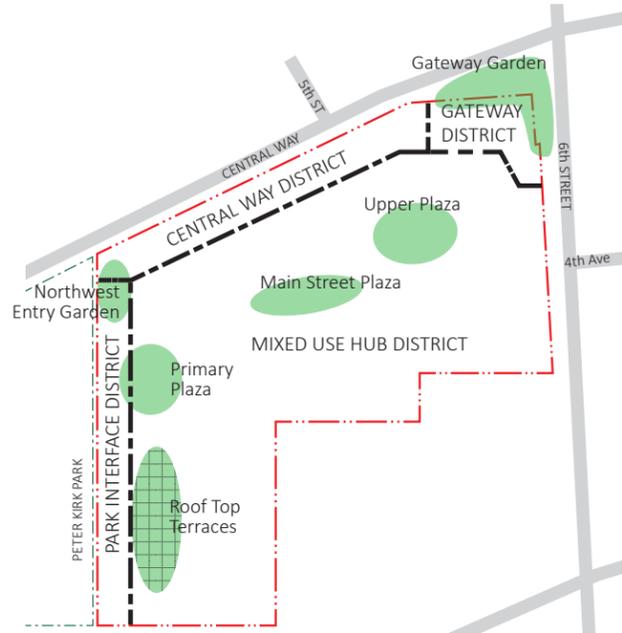


Street bench, plantings, and recessed corner entry



Public Spaces: plazas defined by pathways and buildings include amenities such as water features, sitting spaces, landscaping, and changes in materials, colors, and textures





Possible Organization of Pedestrian/Public Spaces as Related to Districts

- d. Plazas and courtyards should include the following:
 - planters and trees to break up space
 - seating, such as benches, tables, or low seating walls
 - special paving, such as integral colored/stained concrete, brick, or other unit pavers
 - specialty pedestrian scale bollards or other types of accent lighting
 - at least one of: public art and/or water feature
- e. Design spaces to allow for variety and individualization of temporary installations such as: lighting, banners, artwork, etc.

3. ENVIRONMENTAL CONSIDERATIONS

Intent: Optimize pedestrian comfort using natural environmental conditions. Promote a pedestrian- and bicycle-friendly atmosphere.

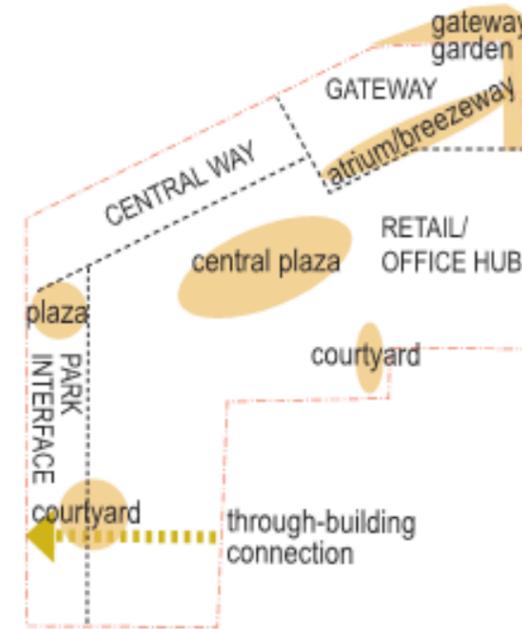
- a. Consider environmental conditions such as sun, shade, and prevailing winds when positioning courtyards and outdoor seating areas. Provide features and amenities to enhance pedestrian and bicycle access throughout the project.

4. PEDESTRIAN CONNECTIONS AND WAYFINDING

Intent: Create a network of safe, attractive, and identifiable linkages for pedestrians.

- a. Provide clearly defined pedestrian connections ~~shall be provided~~ at locations specified in the Pedestrian Spaces and Street Classification sections.
- b. Provide graceful grade transitions - both physical and visual - between upper grade and lower grade levels through the use of: landscaping, terraced planters, overlooking balconies, wide and inviting stairways, and other pedestrian connections.

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2008 Diagram: Pedestrian Space and Districts



Plaza with special paving, seating, planters



Pedestrian and bicycle amenities (left); Wayfinding signage and clearly defined pedestrian connections (center and right)

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5. LIGHTING

Intent: Ensure that lighting contributes to the character of the project, provides personal safety, and does not disturb adjacent developments and residences.

- a. Use city-approved fixtures for street lighting along the city streets.
- b. Lighting elements throughout the project and on adjoining rights of way should be coordinated, including public open spaces, accent lighting, and streets.
- c. Accent lighting along public right-of-way should be soft in character and enrich the pedestrian street life.
- d. Accent lighting within the central pedestrian space should be congruous with the character of the project and **with** the arts and pedestrian space commitments. (See Section 9.)
- e. Lighting should include non-glaring design solutions, such as cut-off fixtures that avoid light spilling over onto other properties.
- f. Flood lighting of entire building facades is discouraged.
- g. Lighting on upper levels should be sensitive to Peter Kirk Park, residences, and drivers.



Integrated lighting enhances architectural character and provides pedestrian safety

Architectural and landscape elements provide screening.



6. SCREENING OF TRASH AND SERVICE AREAS

Intent: To screen trash and service areas from public view.

- a. All service, loading, and trash collection areas shall be screened by a combination of planting and architectural treatment similar to the design of the adjacent building.
- b. Avoid wherever possible locating service, loading, and trash collection facilities in pedestrian-oriented areas.

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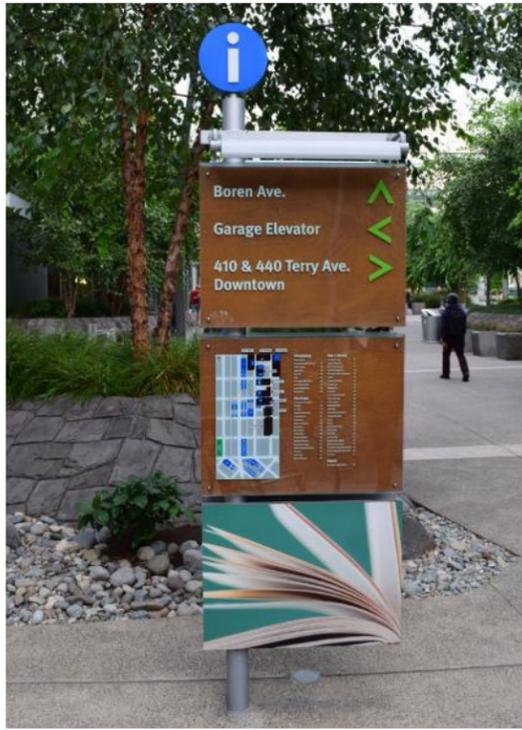
7. SIGNS

Create a Master Sign Plan ~~will be created with the city~~ that is in keeping with the following design objectives:

Intent: Create signs that are creative, engaging, and effective for a variety of user groups and respond to a variety of spaces.

- a. Sign~~sage~~age should be complementary and integrated with the unique character of the specific areas or buildings where they are located.
- b. Sign~~sage~~age should be high quality and consistent with the contemporary urban character of comparable developments in similar regions.
- c. The design of buildings should identify locations, sizes, and general design for future sign~~sage~~age.
- d. The Master Sign Plan should include a hierarchy of elements based on function, such as:
 - site sign~~sage~~age for entries, wayfinding, Parkplace identity
 - building sign~~sage~~age for addressing and landmarking
 - tenant sign~~sage~~age to encourage expressive individualization

A hierarchy of sign functions: site signs for entry and wayfinding (above, left), building signs for landmarking (below left), and tenant signs that express individual character (below center³ and right)



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BUILDING DESIGN

1. ORIENTATION TO THE STREET

Intent: Ensure that buildings contribute to the liveliness of Parkplace's public spaces, and overall community character.

The following design treatments should apply to areas where ~~with required~~ retail frontages occur:

- a. Streets and public spaces should be enlivened by storefronts, windows, merchandise and other activity. Buildings should be designed with frequent entrances to encourage multi-tenant occupancy and walk-in traffic.
- b. Ground level retail heights should be ~~between a~~ **minimum of 14 to 18** feet in height.
- c. Entrances: Principal building entry should be visible from internal or external streets and public space. Entries should be marked by large entry doors **and/or** canopy/portico/overhang.
- d. Transparency: To help provide a visual connection between activities, ground floor facades should provide:
 - windows of clear vision glass (i.e. transparent) beginning no higher than 2' above grade to at least 10' above grade,
 - 60% minimum of facade length along Central Way, ~~P-1, P-2~~ **and the internal Main Street**, should provide transparency,
 - 50% minimum of **retail** facade length along ~~A-1, A-4~~ **access streets from Central Way to the site** should provide transparency.
- e. Weather Protection: To provide pedestrians cover from weather, canopies or awnings should be:
 - a minimum of 5 feet in width unless in conflict with vehicles,
 - placed along at least 75% of facades of retail frontages, and constructed of durable materials,
 - allowed to vary in design,
 - encouraged to have continuity, minimizing gaps.



Retail frontages with wide sidewalks, transparency, visible entries, and weather protection

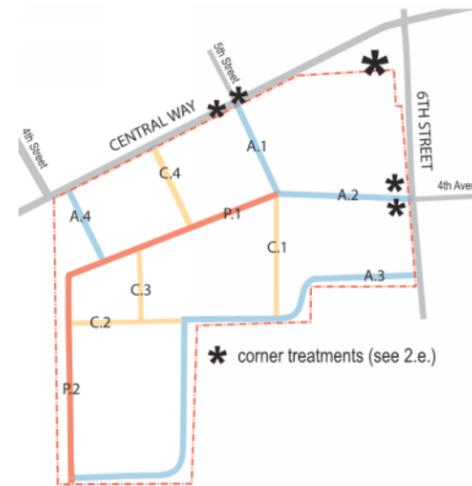


Articulation, massing, and diversity to maintain a pedestrian scale.
(middle: 4)

2. MASSING/ARTICULATION

Intent: Create a variety of form and massing through articulation and use of materials to maintain a pedestrian scale.

- a. In general, break down the scale and massing of larger buildings into smaller and varied volumes.
- b. All building faces should be responsive to the context of the surrounding environment and neighboring buildings.
- c. **Design** all sides of the building ~~shall be designed~~ with care (i.e. there should be no "backside" of a building.)
- d. Buildings should distinguish a "base" using articulation and materials. Include regulating lines and rhythms to create a pedestrian-scaled environment.
- e. Provide clear pattern of building openings. Windows, balconies, and bays should unify a building's street wall and add considerably to a facade's three-dimensional quality.
- f. Ribbon windows and extensive use of mirrored glass are discouraged.
- g. Employ major architectural expressions into the facade, roof form, massing, and orientation, such as tower forms, oversized windows, and entrances to demarcate gateways and intersections. Strong corner massing can function as a visual anchor at key locations within the project area. ~~See diagram for encouraged key locations.~~
- h. Building modulation should be employed to break up long facades and create a visual interest unique to each building in the project. The type of modulation should be determined by the overall design concept for each building, using dimensions from window sizes, column spacing, rain screen paneling, etc. to determine a distinct design solution.
- i. Roof Silhouettes: Express roofs in varied ways. Consider potential views of roof tops from adjacent buildings. Avoid monotonous design.
- j. Locate and/or screen rooftop equipment so that it is not visible from public spaces. Integrate rooftop screening into building's form.



2008 Diagram: Corner treatments

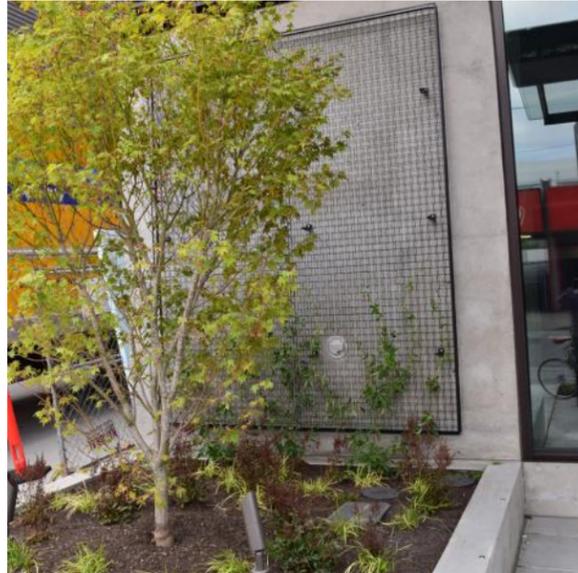
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3. BLANK WALL TREATMENTS

Intent: Reduce the visual impact of blank walls by providing visual interest.

- a. Although blank walls are generally not encouraged along public streets and pedestrian spaces, there may be a few occasions in which they are necessary for functional purposes. Any blank walls longer than 20 feet should incorporate two or more of the following to provide visual interest:
 - vegetation, such as trees, shrubs, ground cover and or vines adjacent to the wall surface
 - artwork, such as bas-relief sculpture, murals, or trellis structures
 - seating area with special paving and planting
 - architectural detailing, reveals, contrasting materials, or other special visual interest



4. ENCOURAGE HIGH-QUALITY DESIGN

Intent: Ensure that all buildings in the project area are constructed as a quality addition to the Kirkland Community.

- a. Exterior architectural design and building materials should exhibit permanence and quality appropriate to an Kirkland's urban setting.



5. BUILDING DIVERSITY

Intent: Ensure that project-buildings in the project are distinct and respond to the unique character of their specific function and location.

- a. Buildings should be designed to integrate with each other, while demonstrating architectural diversity. Buildings should be responsive to each specific district and its site conditions.
- b. Materials should be selected to integrate with each other and to help provide a richness of architectural diversity.
- c. Windows should incorporate variation of patterning between buildings.



Vegetation, art, and screening provide visual interest at blank walls (center image 5)

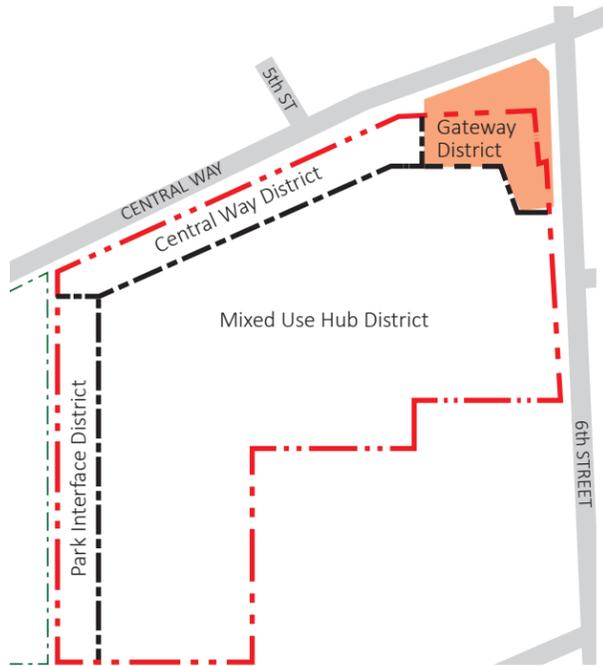
**13. Design Guidelines:
District-Specific**

A. GATEWAY DISTRICT

Intent: Create a welcoming feature to Parkplace and to downtown Kirkland. This area should create an inviting entryway that is representative of the community through the use of art, landscape, and architecture.

SITE PLANNING

1. Incorporation of Triangular Lot "Gateway Garden": Incorporate the northeast triangular lot (excess right-of-way) into the project design to create a distinct gateway entrance that is integrated with the Parkplace development. Include:
 - a. Public Access: Public access into the site should be visible and accessible from the corner of 6th Street and Central Way.
 - b. Hardscape/Vegetation: Paving and landscaping materials should identify pedestrian spaces and access.
 - c. Trees and Other Planting: Landscaping should be of appropriate scale and species to make a significant gateway gesture. Trees should be selected to provide visibility of businesses and maintained to encourage proper growth and height.
 - d. Signage (downtown entry): Incorporate wayfinding signage directing visitors to Downtown, Peter Kirk Park, Waterfront/Marina, City Hall, and Civic District.
2. Public Space Connecting to Triangular Lot: Design of additional public space should be integrated with the triangular lot to provide a congruous pedestrian environment.
 - a. Public Access: Connect pedestrian access to the gateway garden, adjacent streets, and public open spaces.
 - b. Hardscape/Vegetation: Paving and landscaping materials should identify pedestrian spaces and access.
 - c. Seating: Incorporate seating along pedestrian pathways and gathering spaces.
 - d. Artwork: Incorporate art in an appropriate scale to distinguish the significance of this corner.
3. ~~Atrium/Breezeway Space~~ **Pedestrian Connection:** Create a pedestrian connection from the corner of 6th and Central into the heart of the project. (See Section 10.A.) ~~The atrium/breezeway space~~ **This connection** will include the following:
 - a. ~~Covered walkway~~ **Pedestrian weather protection**
 - b. public connection from 6th to ~~central plaza~~ **the interior of the site** open during regular operating hours
 - c. pedestrian lighting
 - d. seating

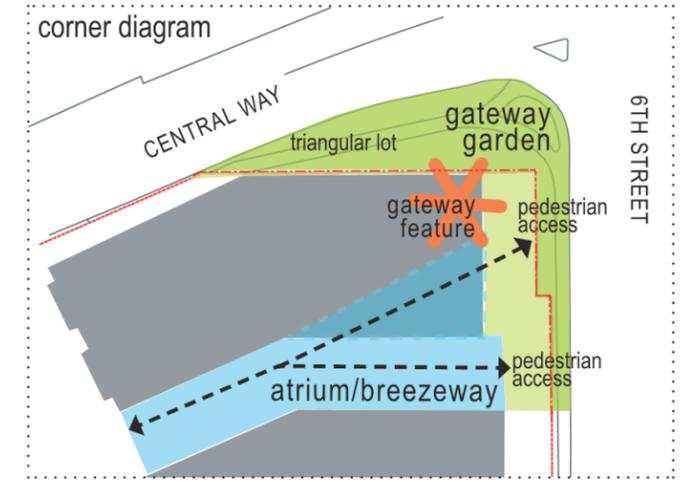


Key Plan: Gateway District



Distinct corner treatments: provide identity for the development and integrate pedestrian hardscape, landscaping, seating, and art

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2008 Diagram: Gateway corner

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and may ~~consider~~ including:

- a. enclosed public space
 - b. retail/restaurant uses
 - c. covered play/activity space
4. Buildings should be separated from or differentiated from each other at this corner so that they are not perceived as one building.

BUILDING DESIGN: BUILDING AS GATEWAY

1. Ground Level Treatment

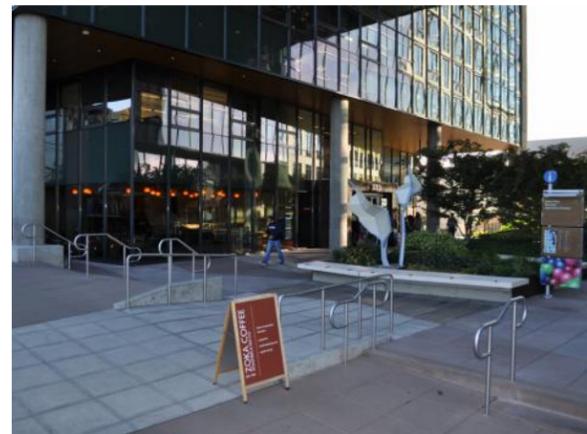
- a. Setbacks from Streets - The ground floor levels of the corner building should be permitted to set back to allow for cut away view and obvious pedestrian connection into the site.
- b. ~~Active and Inviting Retail/Restaurant Uses~~ **Design for an engaging pedestrian experience** ~~retail and restaurant uses~~ along ground floor of the building.
- c. Details Visible at Different Movement Speeds - Incorporate details in the building along the corner that bring visual interest at the pedestrian level, as well as for vehicular traffic entering Kirkland.

2. Upper Levels

- a. Change of Expression/Material Choices: A clear visual division between upper and lower floors should be incorporated through a change in materials, colors, and forms.
- b. ~~Modulation: Building form modulation~~ **Step-backs** ~~A modulated step back should be incorporated to decrease the apparent bulk of the~~ **after the third level** ~~(approximately 50') on building facade along Central Way. This step back can vary in depths from 0-10 feet, so long the upper levels of building appear to be receding from the base. Step backs are measured from the exposed face of the building above grade, not from any property line.~~
- c. Top Floor/Roof Edge: should have a distinct profile against the sky through elements such as projections, overhangs, cornices, step backs, trellises, changes in material, or other elements.
- d. Accent Lighting: The innovative use of accent lighting incorporated into the building facade is encouraged. Lighting should include non-glaring design solutions such as cut off fixtures that avoid light spilling over onto other properties. Flood lighting of entire building facades is discouraged.



Building modulation, clear visual distinction between upper and lower floors, and details visible at different speeds



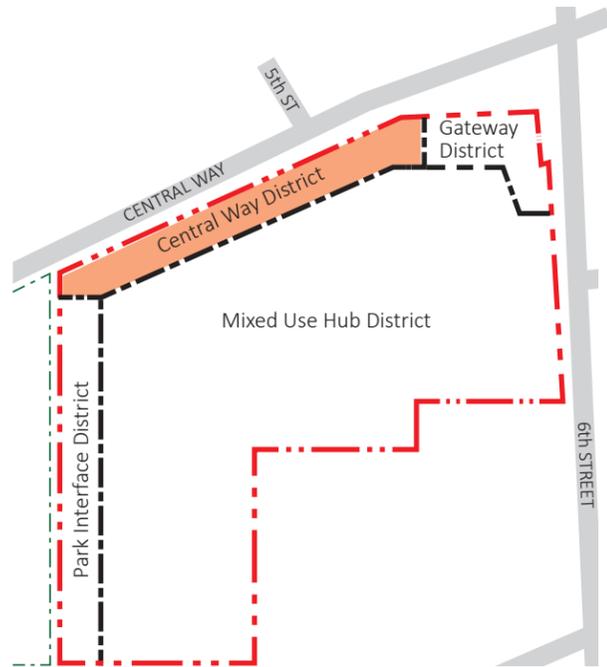
Ground floors set back to provide pedestrian connection to site



Top floor/roof edge provides distinctive profile⁶

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Key Plan: Central Way District

B. CENTRAL WAY DISTRICT

Intent: Respond to Central Way as a major arterial linking downtown Kirkland with areas east and beyond. Parkplace must take advantage of this traffic volume to help create a multi-functioning, pedestrian-scale shopping street that brings visual activity to the street edge.

SITE PLANNING

1. Encourage connections and activate the street edge by incorporating:
 - on-street parking along Central Way
 - buildings located up to the edge of the sidewalk
 - storefront and hotel entrances
 - visibility into buildings in order to engage pedestrian interest
 - generous sidewalk amenity zone (trees, lights, benches)
 - street tree selection and spacing that provide visual continuity, buffer pedestrians from the busy street, and allow visibility of retail
 - pedestrian signage
2. Reduce the length of street wall by pulling back portions of the building at ground level from the street edge in key locations provided street continuity is not interrupted.
3. ~~The limited vehicular access to mid-block connection may be accessed~~ Include a pedestrian-only connection from Central Way and into the interior of the project. Pedestrian access along this route should include pedestrian-scaled lighting and a clear connection to the streetscape/plaza space on the opposite side.
4. Activate building corners with visibility into retail and/or other inviting design features, as denoted on Organization of Uses diagram (page 10).



Pedestrian-only connection from Central Way to interior streetscape/plaza on opposite side?



Building corners articulated with glazing, canopies, and special paving

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BUILDING DESIGN

1. Reduce apparent bulk of buildings along Central Way at pedestrian level. Provide a two story pedestrian pass-through in buildings fronting Central Way. The pass-through should be of sufficient size to provide views into the "main street" retail, and provide articulation in the building mass at grade. ~~by incorporating a 20 foot upper level step back after the third story along the majority of the facade. However in places, step backs can vary in depths from 0-20 feet, so long as the overall upper levels of building appear to be receding from the base. Step backs are measured from the exposed face of the building above grade, not from any property line.~~
2. At the upper levels, reduce apparent building facade length by providing breaks in the building facade or shifts in building mass. Facades that are stepped back should be distinguished by a change in elements such as window design, railings, trellises, details, materials, and/or color so that the result is a richly organized combination of features that face the street.
3. Set back upper floors from the base at the western portion of the office building fronting Central Way. ~~Balconies, terraces, and landscaping are encouraged in upper level step backs.~~



Upper levels set back from base at western edge of office building; ample glazing and canopies enhance pedestrian experience⁸



Two-story pedestrian pass-through to promote physical and visual connections, and to reduce apparent building bulk at grade level



Breaks in building mass reduce apparent bulk of building⁹

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C. PARK INTERFACE DISTRICT

Intent: Create a strong connection from the park and downtown core that allows for clear pedestrian flow to and into the site by incorporating engaging building frontages, plazas, gardens, and other design treatments.

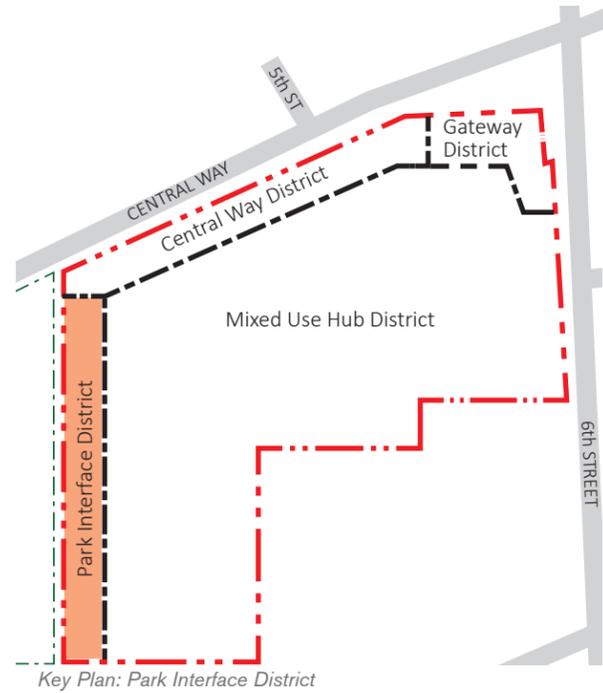
SITE PLANNING

1. Incorporate ample landscaping and distinctive lighting.
2. Incorporate raised crosswalks 20' minimum in width and special paving to promote pedestrian priority along the north-south street bordering the park.
3. Encourage retail spill-out spaces and landscaped courtyards along the building edge. Bring the "indoor" out and "outdoor" in by spilling retail spaces onto the sidewalk and creating small gathering spaces along building edges.
4. Create a visual barrier for drivers between the drive lane and pedestrian walkway along the Peter Kirk Park edge using one or more elements such as: plantings, bollards, small seating walls, stone artwork, etc.

BUILDING DESIGN

1. Buildings shall address park and street by incorporating:
 - terraces and balconies
 - entrances to retail along promenade
 - greater transparency at ground floor or planting zone and/or canopy at edge of buildings where transparency is not feasible, such as theater facades.
 - street front courtyards
 - retail spill-out spaces
2. Where feasible, provide rooftop terraces on lower roof levels as gathering spaces that include amenities such as:
 - seating
 - landscaping
 - canopies or coverings for weather protection
 - public access open during regular operating hours
 - retail/food service where appropriate

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Safe, clearly marked, pedestrian-friendly crosswalks



Pedestrian-oriented park interface: trees, clear markings, ground floor retail, balconies¹⁰

D. MIXED USE HUB

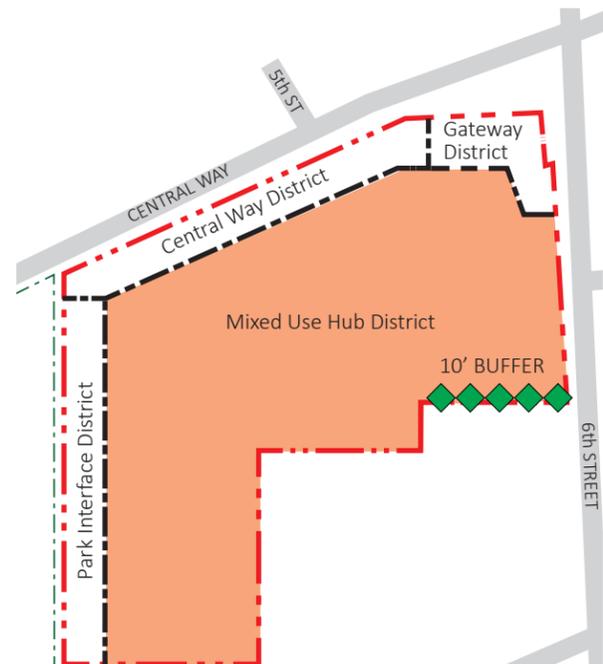
Intent: To establish a vibrant **Mixed Use Hub** with activated public space and retail/window shopping experience with a mix of uses, both connected to and overlooking a common central plaza, primary plaza, and Peter Kirk Park.

SITE PLANNING

- The central plaza space should be integrated visually and physically with their surroundings, and should provide significant gathering and activity spaces by incorporating the following:
 - special paving
 - water feature(s)
 - special landscaping
 - seating: covered and open
 - distinct lighting
 - access to sunlight
 - accommodations for concerts/performance
- The plaza space Plazas should be supported as important activity spaces by surrounding them with active public-oriented amenities such as ground floor retail, restaurants, and cafes.
- Locate plazas at or near street grade to promote physical and visual connection to the street and adjacent buildings and their entrances.
- Design outdoor space with safety in mind; public plazas should promote visibility from the street and provide architecturally compatible lighting to enhance night time security
- A ten foot permanent landscaped edge along the southeast property line adjacent to residential uses should be incorporated within the street design. (See diagram at right.)
- The district should also consider providing:
 - small retail pavilion(s)
 - children's interactive feature
- A through-building pedestrian connection on the southeastern portion of the site should be provided and include:
 - through public 24-hour access during normal business hours
 - connection to Peter Kirk Park
 - pedestrian weather protection and wayfinding signs
 - gracious entries on both sides of the building
 - the design of the interior space should feel public and accommodating, so users do not feel that they are intruding into private space.

NEW DOCUMENT WITH TEXT MARK-UPS

NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED



Key Plan: Mixed Use Hub District showing buffer at southeast property line



Pedestrian courtyards framed by retail use¹¹

DESIGN GUIDELINES

NEW DOCUMENT WITH TEXT MARK-UPS

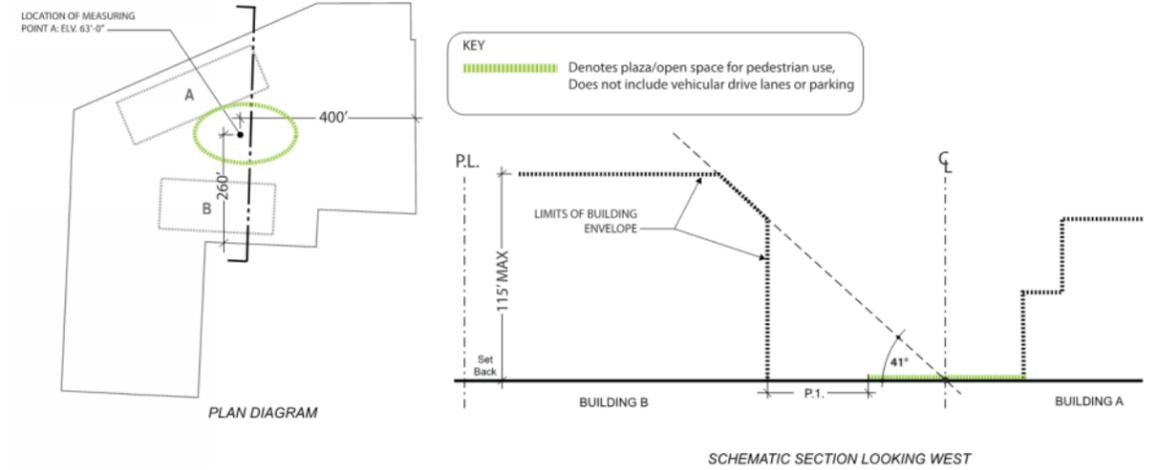
NOTES ON CHANGES & 2008 GRAPHICS TO BE REPLACED

BUILDING DESIGN

1. Lower level facades with predominantly retail uses should locate entrances at the sidewalk or edge of public space to frame pedestrian spaces in key locations.
2. Where feasible, provide rooftop terraces on lower roof levels as gathering spaces that include such amenities as:
 - seating
 - landscaping
 - canopies or coverings for weather protection
 - public access open during regular operating hours
 - retail/food service locations
3. In order to maximize the amount of sunlight in the central primary plaza, building B as depicted in the diagram below buildings to the south should be contained under a line at a 41 degree angle measured from the center of the plaza per diagrams below.
4. Buildings located in the southern most portion of the site should provide generous and substantial modulation in response to their proximity to neighboring buildings including:
 - creating varied edges and visual interest on long and tall buildings
 - employing modulation to visually break up long facades
 - providing patterns of windows, bays, and/or balconies that emphasize changes in modulation



Plazas providing significant gathering and activity space, framed by buildings^{above:12, below:13}



2008 Diagrams: Solar access at Plaza

REMOVE DIAGRAMS: The primary plaza in the new development will be closer to Peter Kirk Park and has no large buildings directly to the south that would impede solar access. The 41° angle requirement will still apply, but will be easily met and requires no diagram.

The Notes section is new to the 2014 document.

Notes

IMAGE CREDITS

CREDITS IN PROCESS

The following sources were used for end-noted images. All other images and illustrations are provided by CollinsWoerman.

1. Kirkland, WA. Map. *Google Maps*. Google, 6 Aug 2014. Web. 6 Aug 2014.
2. VA, Brett. *Outbuilding bike parking Kirkland WA*. 30 Jan 2010, Kirkland, WA, in *Flickr*. https://www.flickr.com/photos/smart_growth/4575869318/in/set-72157623983604822/
3. La Citta Vita. *Courtyard Shopping in Berlin*. 16 Jan 2011, in *Flickr*. <https://www.flickr.com/photos/la-citta-vita/5852199389/>
4. [from 2008/LMN version]
5. La Citta Vita. *Green Wall*. 27 Feb 2012, in *Flickr*. <https://www.flickr.com/photos/la-citta-vita/7802506458/in/photolist-c6gcQA-8fZ64K-8fZ5FT-cTtKq-aHZJqg>
6. [credit in process]
7. [credit in process]
8. [credit in process]
9. [credit in process]
10. La Citta Vita. *Waterfront Architecture*. 27 Feb 2012, in *Flickr*. <https://www.flickr.com/photos/la-citta-vita/7802515158/in/set-72157631113816934/>
11. [credit in process]
12. The Jerde Partnership International. 1999. *You Are Here*. London: Phaidon Press Limited.
13. [credit in process]

THE CITY OF KIRKLAND

Kirkland Parkplace Mixed-Use Development

Master Plan and Design Guidelines

DRAFT

Prepared by CollinsWoerman

December 3, 2014

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Acknowledgment: Written content in this document has been excerpted and/or excerpted and edited from the previously approved *Kirkland Parkplace Mixed Use Development Master Plan and Design Guidelines* document updated December 16, 2008 as created by LMN Architects.

POLICY OVERVIEW

1. Introduction

Located along Central Way and 6th Street, Kirkland Parkplace has the potential to offer many great amenities to Kirkland's downtown. Parkplace is a 501,000 square-foot property defined as CBD-5A in Kirkland's Zoning code. The proposed mixed-use center includes approximately 1.14 million square feet of development consisting of retail, office, residential, and entertainment uses that are, in effect, an extension of the existing downtown.

PURPOSE

This document includes three major parts: (1) a Policy Overview that establishes a vision, procedure, and design intent; (2) a Master Plan comprised of Development Standards that establish basic programming and site planning requirements; and (3) Design Guidelines that establish detailed design standards for the site and buildings.

These Standards and Guidelines provide structure to help meet the goals outlined in the Comprehensive Plan. A discussion of relevant Comprehensive Plan directives and this document's associated responses can be found in Section 7: *Comprehensive Plan Design Direction*.

PROJECT NAMING

While this document references the site's current name of "Kirkland Parkplace", the property owner may choose to re-brand the development and re-name it to reflect its new brand identity.

2. Vision

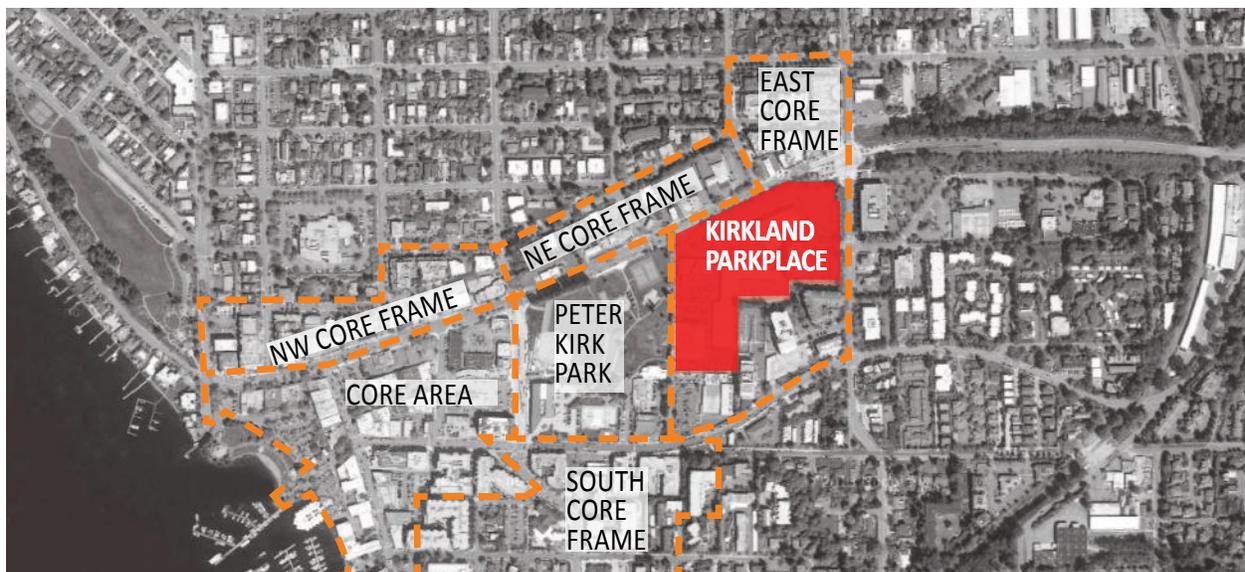
The Kirkland Parkplace Master Plan envisions a transformation of the existing suburban style office park and retail area to a lively, integrated mixed-use center.

Parkplace creates a new destination in Kirkland featuring tree-lined streets, landscaped open spaces, offices and residences overlooking public plazas, and a wide variety of shopping, dining, entertainment, and recreation experiences. Parkplace's contemporary Northwest architecture evokes Kirkland and its environs with green design, appropriate massing, and orientation. Appropriate placement of trees, fountains, benches, street lamps, and decorative sidewalk treatments add a rich texture to Parkplace's plazas and streets.

The combination of pedestrian-oriented streets, distinctive architecture, unique urban character, sensitive integration and progressive sustainable design strategies will make Kirkland Parkplace an attractive and valued gathering place for Kirkland's citizens.

The compact design includes a diversity of spaces for gathering and bustling activity, while maintaining a human scale. This reflects and celebrates the evolution of Kirkland: balancing the need for growth and economic opportunity, but not losing touch with the comfortable, small-town roots of its past.

Kirkland Parkplace is both a home and a destination.



Kirkland Parkplace: Design District 5A, part of the East Core Frame in Kirkland's downtown area¹

3. Application

The Master Plan and Design Guidelines set forth in this document have been created to guide the development of Kirkland Parkplace to meet the intent of the vision for CBD-5A of the City of Kirkland. This Master Plan and Design Guidelines Document allows increased height and reduced setbacks in exchange for providing a mixed-use center and public amenities. These Standards and Guidelines are to be used in addition to the standard zoning regulations for CBD-5A. They are supplemental, not a substitution, to the City of Kirkland Municipal Code and supporting documents.

4. Review Process: Determining Compliance

This document establishes performance criteria and provides recommendations for achieving specific design objectives. Compliance with the Master Plan, including general standards; general public amenity, and access locations; organization of uses; and street dimensional requirements shall be determined by administrative review (planning official). Compliance and consistency with the Design Guidelines shall be determined by the Design Review Board in accordance with KMC 142.35.9. In the DRB's review of the project, the Board shall respect the requirements and commitments established in this Master Plan.

6. Phasing

Depending on market conditions, this development will be staged in three major phases (A, B, and C) with two minor stages in phase A (A1 and A2).

Each independent phase will be designed and built to ensure that, at completion, there are no unsafe or unsightly temporary conditions and that pedestrian connections to and through the site are maintained and/or restored.

As a condition of design review approval for each phase, the applicant shall demonstrate how these conditions will be satisfied for that phase.

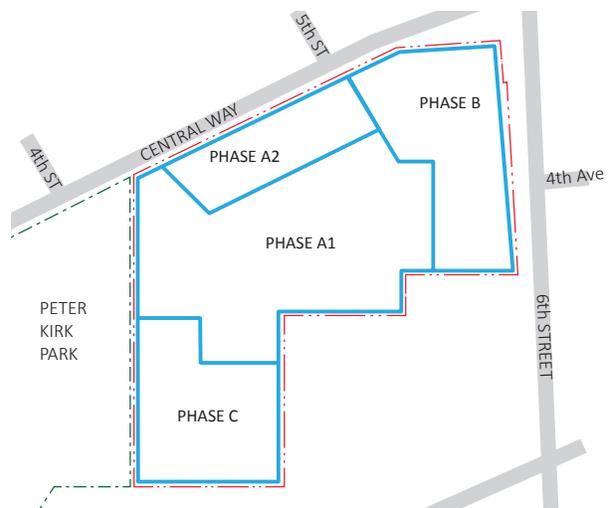
5. Modifications

A major modification to the Master Plan is any proposal that would result in a change that would substantially alter the Plan's proposed development such as: decrease in open space quantity, changes to locations of primary and secondary internal streets, or changes in allowed use. Major modifications to the Master Plan shall require a staff review for consistency with the Comprehensive Plan and City Council approval. (Refer to KMC 3.30.040.)

A minor modification to the Master Plan, reviewed by the Planning Director, is any proposal that would result in a change that would not substantially alter the Plan's proposed development such as: facade treatments, street design variation, character/design detail of public spaces, or minor variations in design of sidewalks, pathways, lighting, and landscaping.

The Design Review Board may grant a design departure or minor variation in the Design Guidelines only if it finds that both of the following requirements are met:

- a. The variation is consistent with the intent of the guideline and results in superior design.
- b. The departure will not result in any substantial detrimental effect on nearby properties or the neighborhood.



Project phasing by location. Sequence of phasing to be determined.

DRAFT KIRKLAND PARKPLACE MIXED-USE DEVELOPMENT: MASTER PLAN & DESIGN GUIDELINES

7. Comprehensive Plan Design Direction

The City of Kirkland's Comprehensive Plan, Section XV.D, includes several policies and guidelines directly related to the Parkplace site. Four relevant Comprehensive Plan directives and associated responses are included below:

A. CP Policy: *Heights of up to eight stories are appropriate as an incentive to create a network of public spaces around which is organized a dynamic retail destination (CP XV.D-13).*

Response: Parkplace is an urban, open-air retail, restaurant, entertainment, office, and residential complex. (See Section 10 for standards regarding networks of open space, retail frontage, and pedestrian connections.)

B. CP Policy: *Special attention to building design, size, and location should be provided at three key locations:*

- *at the intersection of Central Way and Sixth Street to define and enhance this important downtown gateway;*
- *along Central Way to respond to the context along the north side of street;*
- *and facing Peter Kirk Park to provide a transition in scale to downtown's central green space (CP XV.D-14).*

Pedestrian Connections to adjoining streets, Peter Kirk Park, and adjoining developments should be incorporated to facilitate the integration of the district into the neighborhood (CP XV.D-13).

Response: Specific design guidelines have been defined to encourage unique environments and experiences in each of these three locations. The development standards define pedestrian connection requirements.

C. CP Policy: *Because of the intensity of land use in 5A, the design of the buildings and site should incorporate aggressive sustainability measures, including low impact development measures, deconstruction, green buildings, and transportation demand management (CP XV.D-14).*

Response: The compact development, pedestrian-friendly, mixed-use nature of the land use in CBD-5A is fundamentally sustainable. It provides a live-work balance in downtown Kirkland and provides access to goods and services people need in proximity to where they live. Combined with a commitment to sustainable strategies in the design of the development, Kirkland Parkplace will significantly contribute to lowering carbon emissions and energy use relative to a suburban model of development.

D. CP Policy: *Residential development could be designed to integrate into both the office/retail character of the zone and the active urban nature of Peter Kirk Park (CP XV.D-14).*

Response: The Development Standards provide for up to 30% of building floor area to be devoted to residential use. The proposed residential component will enhance Parkplace's public and retail experience and bring after-hours activity to the development. Residents will have access to a range of services and a direct connection to Peter Kirk Park - all within walking distance.

8. Design Intent

This Master Plan and Design Guidelines document was created using the identified nine Guiding Principles for the project which were derived from input from the City staff, Design Review Board, Planning Commission, various community groups, and the residents of Kirkland.

1. Emotional ownership by the community:
 - Incorporate the project into the story of Kirkland.
 - Enable meaningful community exchanges.
 - Inspire unique experiences and discoveries.
 - Promote the coalescence of Community, Culture, and Commerce.
 - Provide a 'transforming experience' vs. a 'transactional experience'.
 - Include neighborhood retail.
2. Site planning connections:
 - Include public spaces such as plazas.
 - Create clear vehicular access and parking.
 - Create strong emphasis on the streetscape.
 - Support active public spaces.
 - Provide clear and inviting public access.
 - Provide connections to Peter Kirk Park.
3. Create community gathering spaces:
 - Create easily accessible public spaces.
 - Develop spaces that vary in size and offer choices for all ages.
 - Provide safety and comfort.
 - Integrate into the social life of downtown Kirkland.
4. Enhance the pedestrian environment:
 - Promote walkability: network of internal and external pedestrian connections.
 - Create visual interest along the street.
 - Incorporate a rich variety of materials.
 - Provide and enhance pedestrian circulation and retail continuity.
5. Integrate motor vehicle access and parking
 - Minimize the visual presence of parked cars.
 - Allow parking to be utilized during nights/weekends for benefit of community and downtown.
6. A mix of uses = a mix of building types:
 - Create a variety of building types, scales, and materials.
 - Express a three-dimensional quality to the public spaces.
7. Appropriate massing and scale:
 - Create pedestrian spaces with access to sun.
 - Address surrounding edges.
 - Consider scale, massing, and detail of individual buildings.
 - Express human-scale, detailed street level building facades.
8. Sustainability:
 - Establish macro-scale/site sustainable strategies.
 - Pursue building-specific sustainable strategies.
 - Encourage tenant-specific sustainable strategies.
9. Mixed-use development:
 - Provide a residential component to the project that will support the viability of a 24-hour development and complement the other uses on the site.



Children's play area at Peter Kirk Park²

MASTER PLAN: DEVELOPMENT STANDARDS

9. Program Requirements

The following requirements and ratios are established to quantify use types at the completion of the project, and are not a requirement for any single phase.

A. PEDESTRIAN SPACE

The development will include a variety of public open spaces that vary in size and character. A minimum of 10%, or 50,000 square feet, of the site shall be activated pedestrian-oriented space, in the form of courtyards, plazas, etc. See diagram (Section 10.D) for approximate locations and dimensional requirements of specific spaces. Definition of appropriate design treatments are found in the district-specific design guidelines (Section 13).

B. ARTS COMMITMENT

In an effort to encourage integrated art into the project, Parkplace is working in collaboration with representatives from the cultural council and local art community and will identify and create opportunities to integrate art into the project.

C. GREEN BUILDING COMMITMENT

Section V. Natural Environment of the Comprehensive Plan outlines broad goals and policies related to environmental sustainability. Section XV.D of the Comprehensive Plan and Guiding Principle #8 (see Section 8 of this document) describe goals specific to the Parkplace site.

1. In response to these goals and policies, the following requirements will apply to the Kirkland Parkplace project:

- a. All new office buildings will be designed achieve a LEED CS Gold threshold. A USGBC Pre-Certification Application showing points meeting LEED CS Gold will be included with permit submittals to show which points will be pursued.
- b. The multi-family residential building(s) will be designed to a LEED for Homes Multifamily Mid-Rise Silver threshold; or to meet Built Green 4 Star certification.

- c. The applicant shall encourage all potential tenants for Kirkland Parkplace to pursue LEED-CI. To accomplish this, the applicant will create and distribute to tenants a set of Tenant Design Guidelines to show strategies tenants can use to achieve LEED-CI certification. These Tenant Design Guidelines will be made available to the City of Kirkland to inform their ongoing sustainability programs.
 - d. At the end of tenant build-outs of the office space, the applicant shall prepare an executive summary for the City of Kirkland, outlining what sustainability measures were incorporated in the tenant build-outs (unless otherwise restricted by tenant confidentiality).
 - e. In addition, the applicant shall strive to make design choices in its Core and Shell buildings that are conducive to the achievement of LEED-CI by tenants.
2. In the interest of promoting a holistic sustainability approach, the applicant shall strive to integrate site-specific strategies identified as focus areas, such as:
- a. Energy efficiency strategies, like centralized cooling options and heat recovery.
 - b. Low Impact Development (LID) strategies like stormwater planters, vegetated roofs, and bioswales.
 - c. Materials and resource strategies like recycled materials, regional materials, and FSC certified wood.

D. COMMUNITY-SERVING RETAIL AND SERVICES

Include neighborhood-serving retail and services. Possible examples include: grocery, childcare, bookstore, drugstore, dry cleaner, movie theater, barber-shop, shoe repair, etc.

E. PARKING

To guide the transformation described in the Comprehensive Plan from “an auto-oriented center surrounded by surface parking into a pedestrian-oriented center integrated into the community” (CP XV.D-13), the majority of parking for the development shall be placed underground. Surface parking will be provided along selected internal streets and at other selected surface parking locations to support retail uses.

TOTAL SITE AREA = 501,000 sf = 100%

SITE AREA BREAKDOWN

Building Footprint 38 - 43%	Open Space* 30 - 35%	Vehicle Areas 22-27%
---------------------------------------	--------------------------------	--------------------------------

*Open Space refers to all at-grade site area other than building footprints, streets, or parking areas. The 50,000 square foot requirement applies specifically to *Pedestrian Space*, outlined below, and in Section 10D: *Pedestrian Space*.

OPEN SPACE BREAKDOWN

Sidewalks 20-26% of Site	Plazas/Courtyards/Gardens ≥ 50,000 sf or 10% of site	Public Roof Terrace 10,000 sf	Private Roof Terrace 10,000 sf
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BUILDING USE BREAKDOWN · Approximate 1,140,000 GROSS SF TOTAL

Commercial Office 600,000 - 615,000 sf	Retail / Fitness / Entertainment 210,000 - 225,000 sf	Residential 250-300 units 280,000 - 300,000 sf
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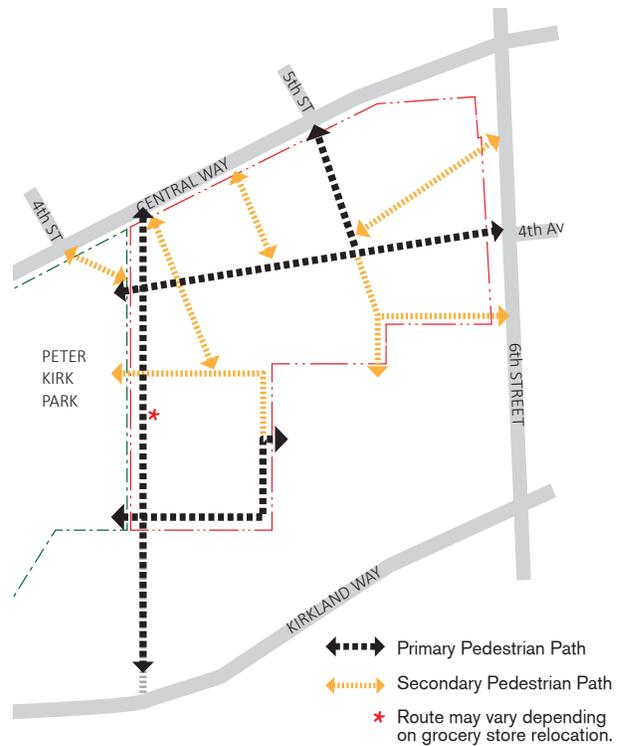
10. Public Amenities, Access, and Organization of Uses

A. PEDESTRIAN CONNECTIONS

Intent: Create a network of identifiable linkages into and through the project site for pedestrians.

The diagram at right shows approximate pedestrian connections. Darker lines indicate primary connections designated by the Comprehensive Plan. Lighter lines show secondary connections linking existing proposed streets as well as Peter Kirk Park. These connections are for public use.

The applicant shall work with the City to define appropriate wayfinding strategies between the development and the Cross Kirkland Corridor.



Network of pedestrian connections

B. RETAIL/RESTAURANT FRONTAGE

Intent: Encourage and contribute to the liveliness and activation of pedestrian-oriented streets and spaces by providing retail and activating uses at ground level.

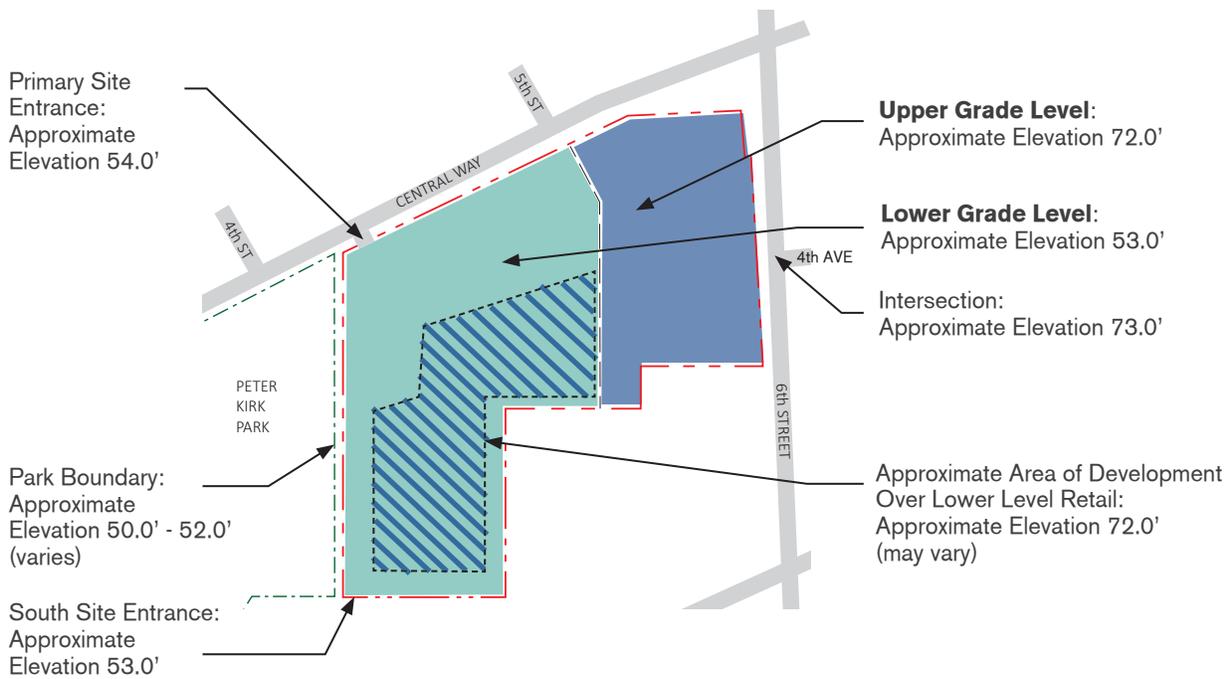
Predominant retail and other pedestrian-encouraging uses, including shops, restaurants, grocery, and a movie theater are encouraged along pedestrian-oriented streets and public spaces. Additional activating uses are encouraged on the ground level throughout the development where feasible.

C. ORGANIZATION OF USES

Intent: Locate building and other uses to support the development goals of the project, including: ground floor retail, upper floor office space, residential space, and public gathering spaces between buildings.

The following diagrams describe the general approximate locations of various building use types, pedestrian connections, parking, and public gathering spaces.

The key plan below illustrates the two grade levels for the site: *Upper Grade Level* and *Lower Grade Level*. The Upper Grade Level relates to the existing street grades at the intersection of 6th Street and 4th Avenue. The Lower Grade Level relates to Peter Kirk Park and the grades at the primary site entrance on Central Way.



Key plan for levels on following two diagrams.



C. ORGANIZATION OF USES: LOWER GRADE LEVEL

BUILDINGS

- A** Retail with entries accessed from internal street;
Office above
- B** Retail and Grocery with entries accessed from internal street and/or open space;
Residential above
- C** Retail and/or Entertainment;
Office above

SITE

- D** Below-Grade Parking
- E** Retail Surface Parking
- F** Pedestrian Space:
Plaza/Courtyard/
Garden

..... Vehicular and Pedestrian Circulation

..... Pedestrian-Only Circulation

▶ Vehicle Site Access

▶ Pedestrian Site Access;
Locations to be Determined

▶ Provide visibility into retail or other activating uses at these locations

● Retail

● Outdoor Amenity

● Parking



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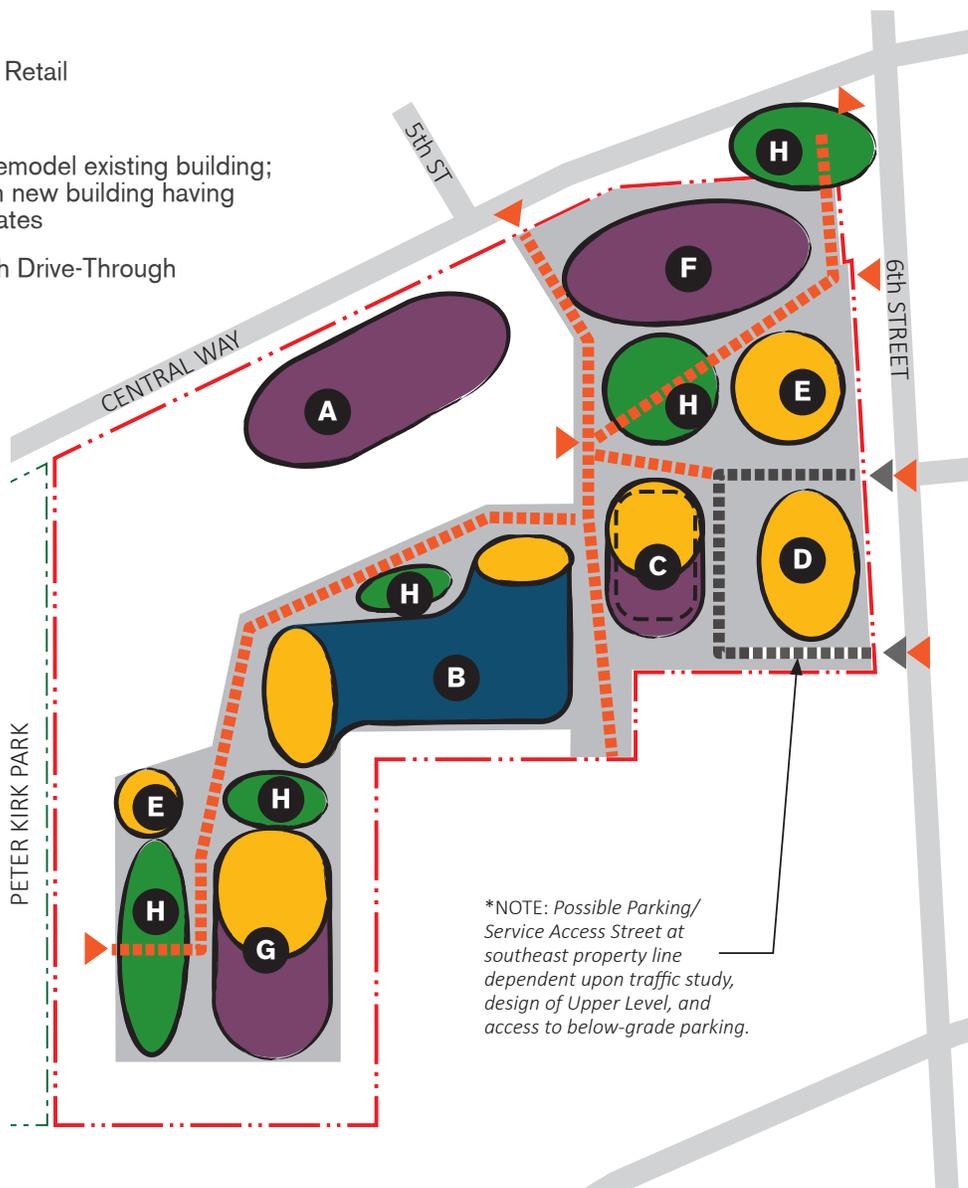
C. ORGANIZATION OF USES: UPPER GRADE LEVEL

BUILDINGS

- A** Office over Lower Level Retail
- B** Residential with Ancillary Retail
- C** Office with Retail;
Options: 1) Retain and remodel existing building;
2) Replace with new building having larger floorplates
- D** Retail: Possible Bank with Drive-Through
- E** Retail
- F** Office
- G** Office with Retail

SITE

- H** Pedestrian Space: Plaza/Courtyard/Garden and/or Roof Terrace
- Vehicular and Pedestrian Circulation
- Pedestrian-Only Circulation
- ▶ Vehicle Site Access
- ▶ Pedestrian Site Access
- Retail
- Outdoor Amenity
- Office
- Residential



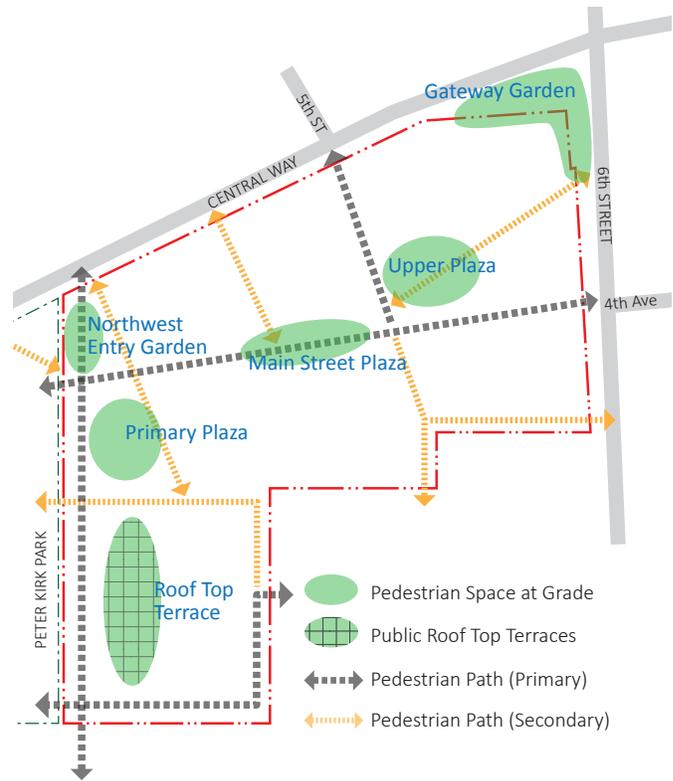
D. PEDESTRIAN SPACE

Intent: Provide a functional and diverse pedestrian environment by creating a variety of usable pedestrian open spaces.

The following types of public/pedestrian space are to be provided at a minimum of 10% of the total lot area, or 50,000 square feet. Locations are approximate and not limited to those shown on the diagram at right.

- a. Primary plaza:** shall have a minimum area of 12,000 square feet with a minimum average width of 70 feet.
- b. Main Street plaza:** a linear sequence of pedestrian spaces along Main Street retail shall have locations with a minimum 35-foot width.
- c. Upper Plaza:** shall include a combination of landscaping and hardscaping with a minimum area of 10,000 square feet.
- d. Northwest Entry Garden:** shall be predominantly landscaped and an extension of Peter Kirk Park.
- e. Smaller courtyard/plazas:** shall have a minimum area of 2,500 square feet each. (not illustrated in diagram at right)
- f. Roof top terraces:** shall provide a minimum of 10,000 square feet total of publicly accessible rooftop terraces in one or more locations.

See district specific guidelines for design parameters of public space (ex. plazas, Section 13.D).



Distribution of pedestrian spaces: along paths, between buildings, and on rooftop terraces. Locations are illustrative and subject to change.

11. Street Classification

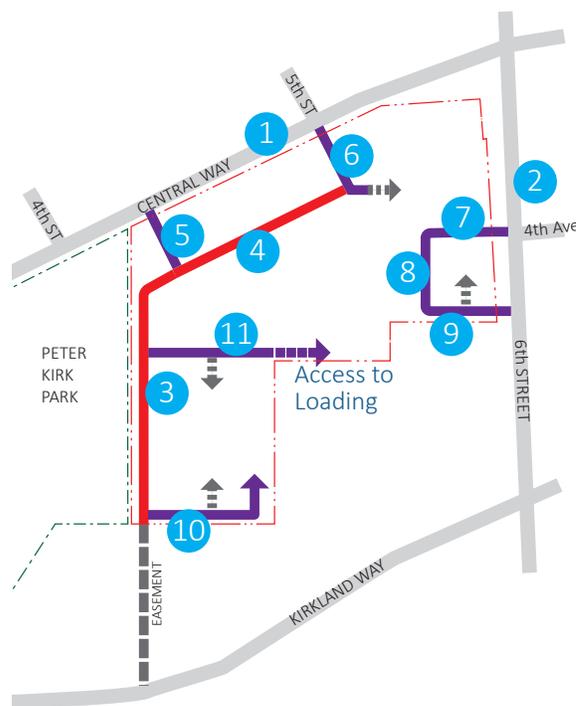
Intent: Create a street and sidewalk network that responds to the existing Kirkland grid pattern, creates a pedestrian-oriented environment, and allows for direct interaction with Peter Kirk Park.

The following street classifications and diagrams represent the various types of streets and approximate locations anticipated in the project. Final location and classification of streets may be adjusted in the final design to include such design techniques as: tight turning radii to calm traffic, curb bulb outs, textured crossings, etc. Access shall be in compliance with city codes and policies for public improvements and emergency access.

Street classifications are meant to be typical sections of the roadway. Slight variations may be necessary to accommodate driveways, street furniture, structural constraints, etc.

Planting adjacent to parking or drive lane may consist of tree wells level with sidewalk or planting strips which are flush with sidewalk or raised above sidewalk. Where tree wells occur, provide minimum 12'-0" total sidewalk width including tree wells, with minimum sidewalk width of 8'-0" and tree grate width of 4'-0" (except as noted on street sections).

Where continuous planting strips are provided in lieu of street tree wells, provide minimum 10'-0" sidewalk and 4'-0" minimum planting strip (unless noted otherwise).



➡ Indicates Possible Access to Below-Grade Parking

ADJACENT PUBLIC STREET IMPROVEMENTS

- 1 Central Way
- 2 6th Street

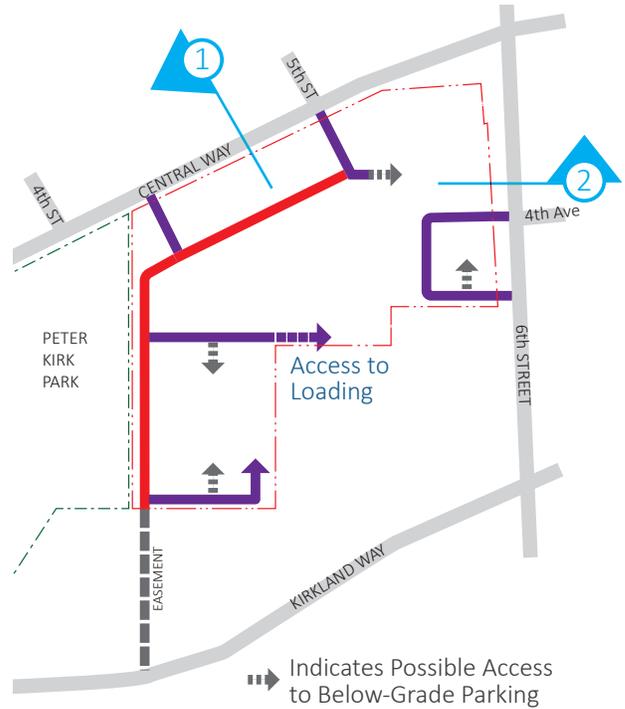
PRIMARY INTERNAL STREETS

- 3 Park Promenade
- 4 Main Street

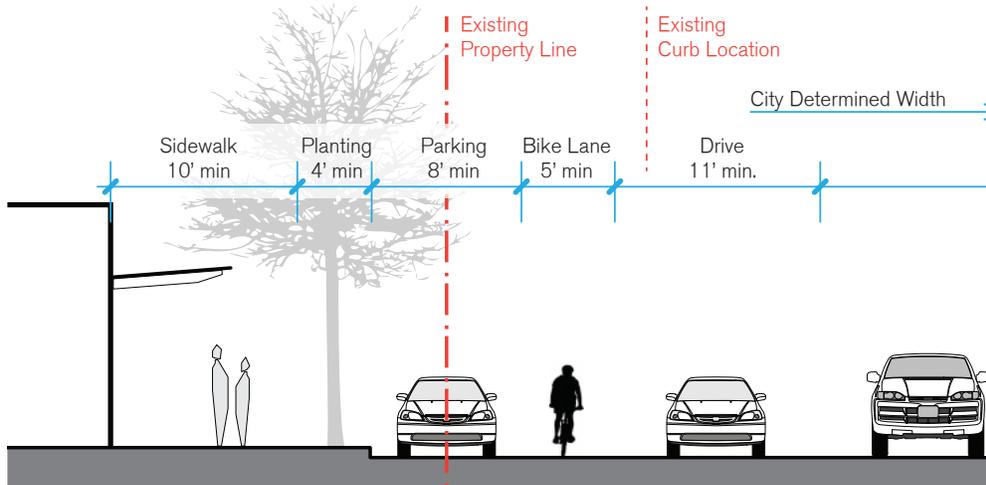
SECONDARY INTERNAL STREETS

- 5 Access Street at Central Way near 4th Street
- 6 Access at Central Way near 5th Street
- 7 Access at 6th Street
- 8 Upper Level Internal Street
- 9 Possible Parking/Service Access at 6th Street
(Dependent upon traffic study, design of Upper Level, and access to below-grade parking)
- 10 Access Street at Southern Property Line
- 11 Parking/Service Access

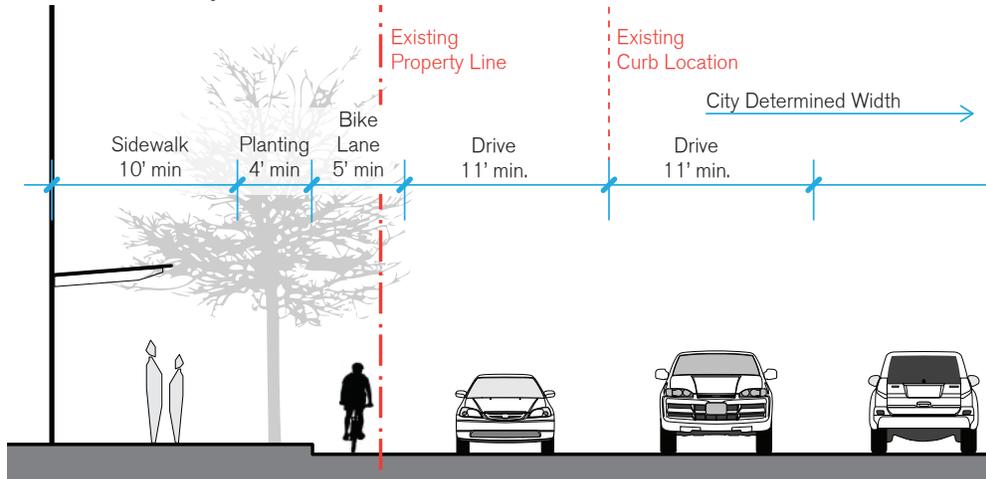
ADJACENT PUBLIC STREET IMPROVEMENTS



1 Central Way Frontage Section (typical)

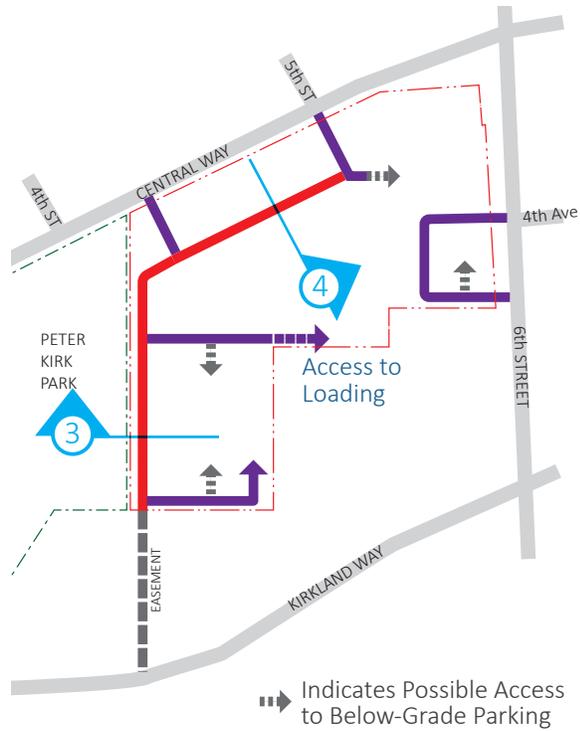


2 6th Street Frontage Section (typical north of 4th Avenue)
(Confirm with City of Kirkland)

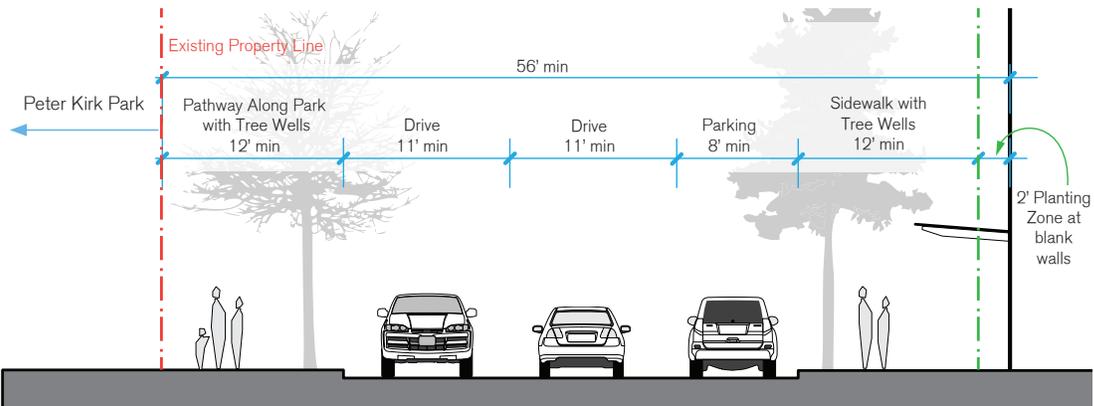


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PRIMARY INTERNAL STREETS

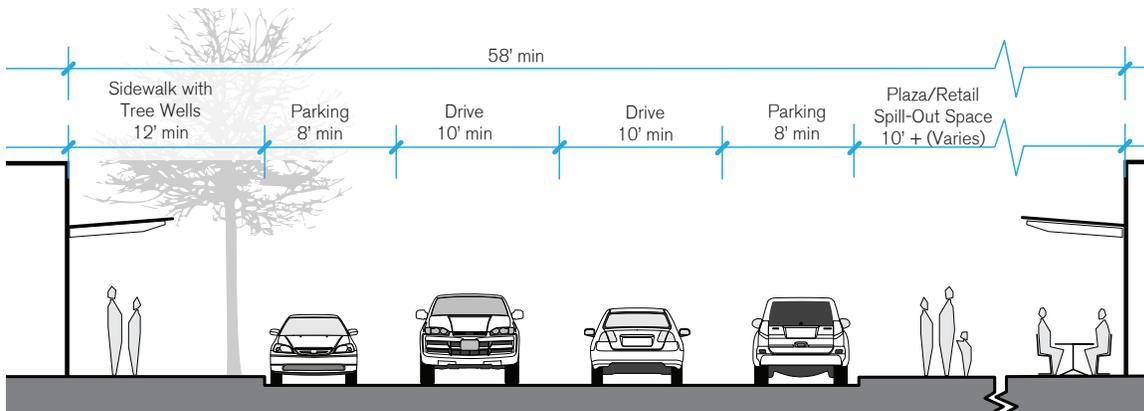


3 Park Promenade Section (typical)



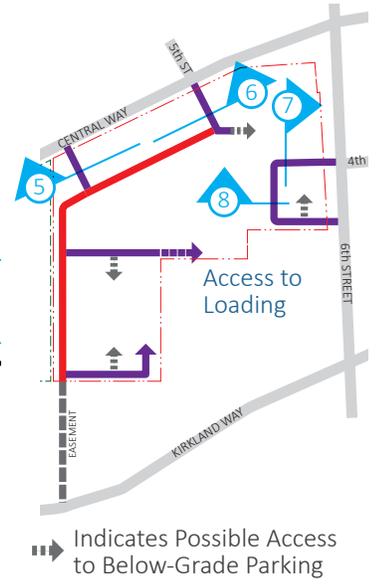
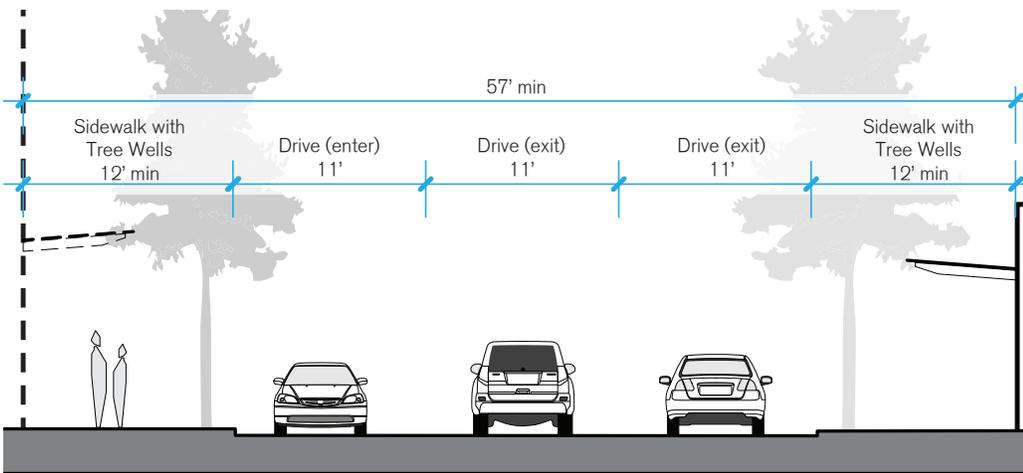
The existing easement to the south shall include a pedestrian sidewalk connecting the Park Promenade with Kirkland Way.

4 Main Street Section (typical)

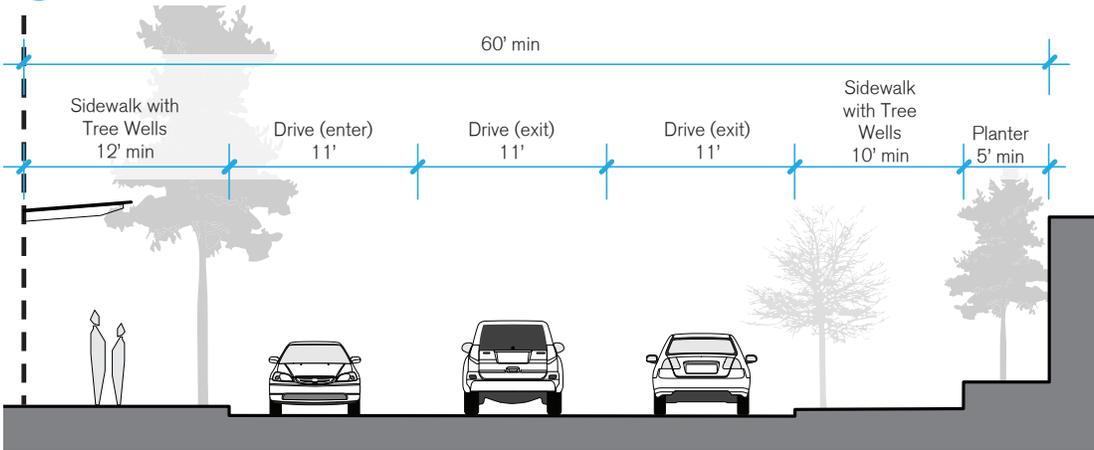


SECONDARY INTERNAL STREETS

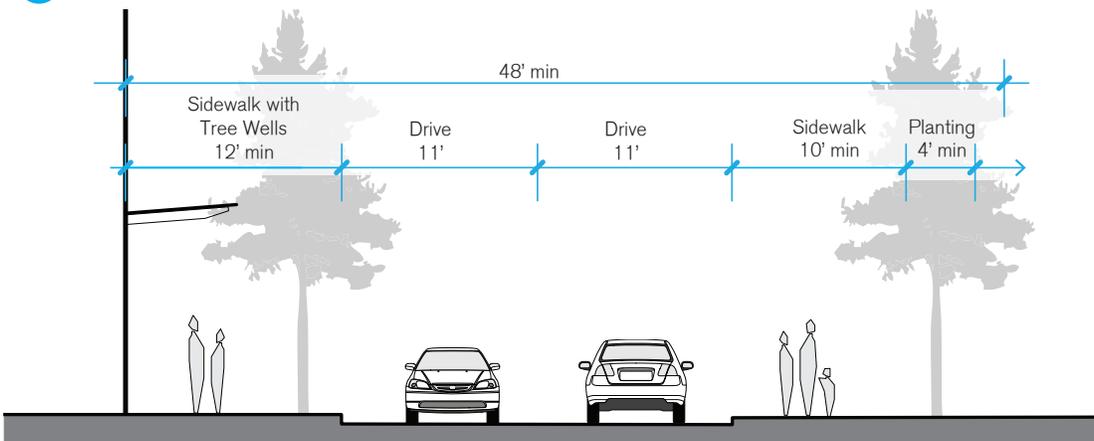
5 7 Access Streets at Central Way, 6th Street (typical)



6 Access Street at Central Way and 5th Street (typical)



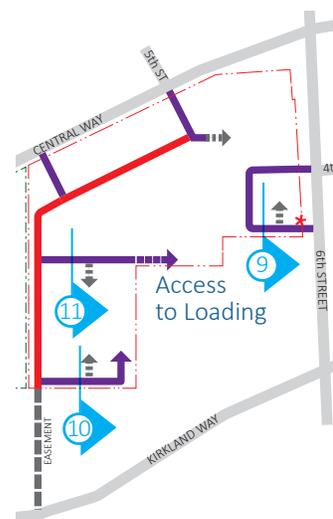
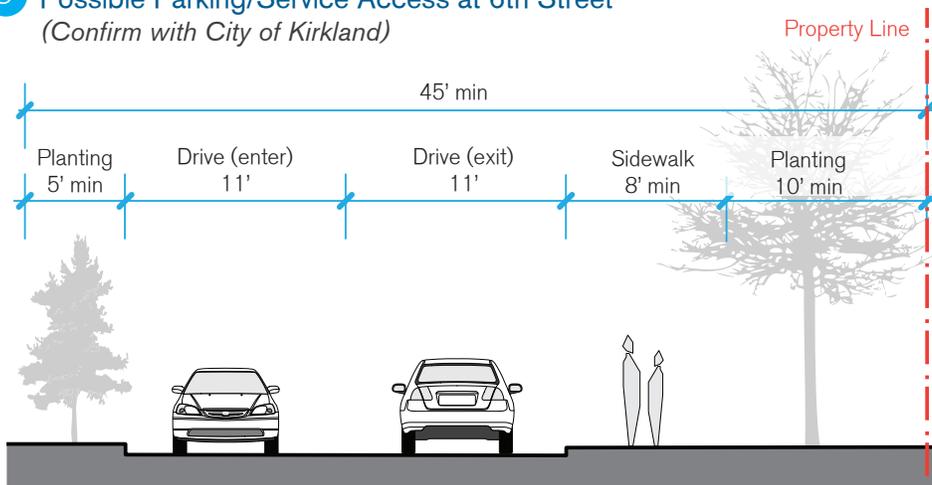
8 Upper Level Internal Street (typical)



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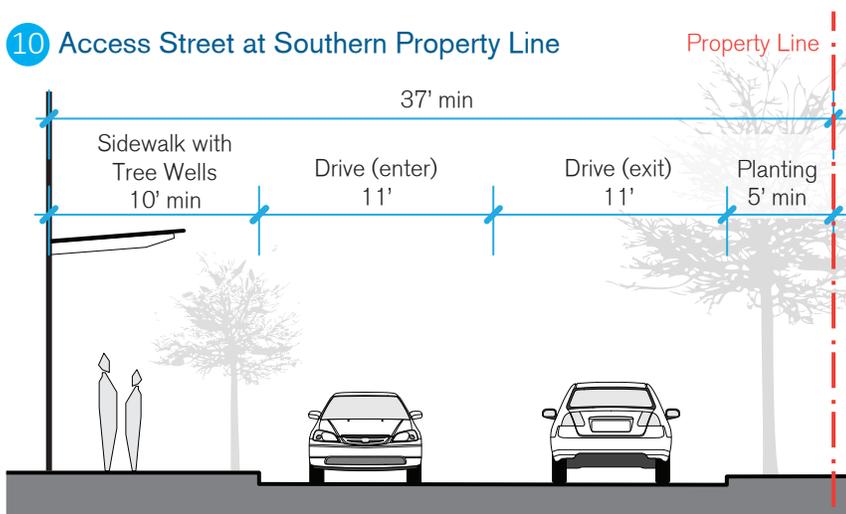
SECONDARY INTERNAL STREETS

9 Possible Parking/Service Access at 6th Street (Confirm with City of Kirkland)

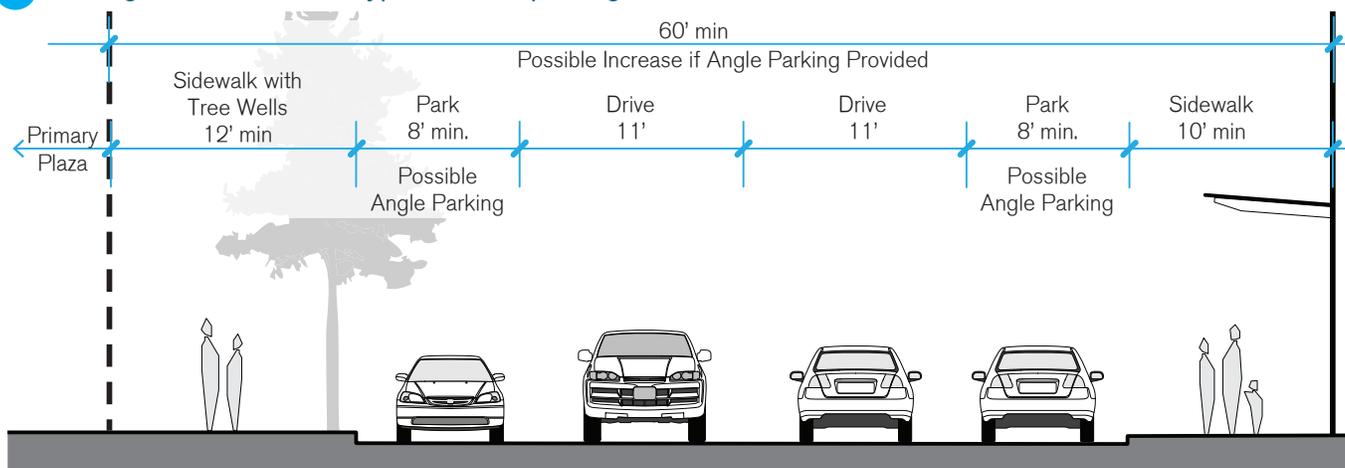


- ➡ Indicates Possible Access to Below-Grade Parking
- * Access Point T.B.D. per Traffic Study

10 Access Street at Southern Property Line

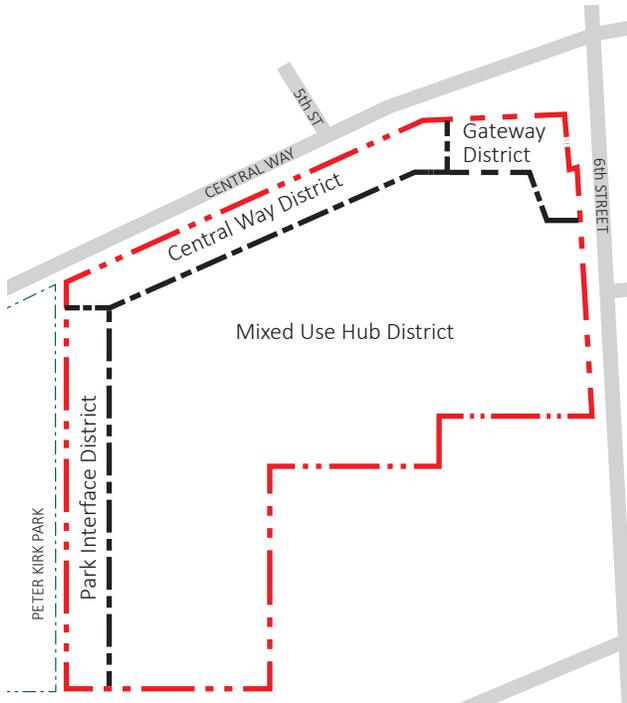


11 Parking/Service Access (typical where parking occurs)



DESIGN GUIDELINES

The Guidelines in Section 12 apply to all districts. Section 13 identifies Guidelines that are district-specific and respond to key locations defined in the City's Comprehensive Plan as requiring special attention. These design districts are defined in the diagram at left.



Key Plan: on-site district locations



Pedestrian-friendly character: on-street parking; amenity zone with street trees, signs, light fixtures; wide sidewalk to accommodate outdoor seating.

12. Design Guidelines: All Districts

Overall Intent: Create a rich pedestrian-oriented environment and successful mixed-use center.

SITE PLANNING

1. STREETScape

Intent: Maintain a continuous and safe streetscape with a pedestrian-friendly character.

- a. Sidewalks should maintain at least an 8 ft clear zone for pedestrian travel (except as noted in street sections).
- b. All streets should contribute to the physical safety and comfort of pedestrians. Provide the following where feasible to help define the sidewalk space:
 - on-street parking (see street classifications)
 - a well-defined amenity zone set to the curb for plantings, street trees, benches, trash receptacles, signs, etc. (Minor deviations for street trees and major planting spaces may be necessary in some spaces due to structural constraints.)
 - wide enough sidewalk space to accommodate outdoor seating where restaurants are anticipated
- c. Use design elements such as separate storefronts, pedestrian-oriented signs, exterior light fixtures, awnings and overhangs to add interest and give a human dimension to street-level building facades.
- d. In general, buildings with active ground floor uses should be set as close as possible to sidewalk to establish active, lively uses. Maintain a continuous street wall, limiting gaps to those necessary to accommodate vehicular and pedestrian access.
- e. Encourage recessed main building and/or shop entrances consistent with a traditional "main street" design that is inviting and promotes streetscape continuity.

- f. The corners of buildings located at street intersections may recess to promote visibility and allow for a collection of people.
- g. Allow larger buildings to recess from the sidewalk edge to allow for entry forecourts, provided street continuity is not interrupted along the majority of the block.

**2. PUBLIC SPACES:
 PLAZAS, COURTYARDS, GARDENS**

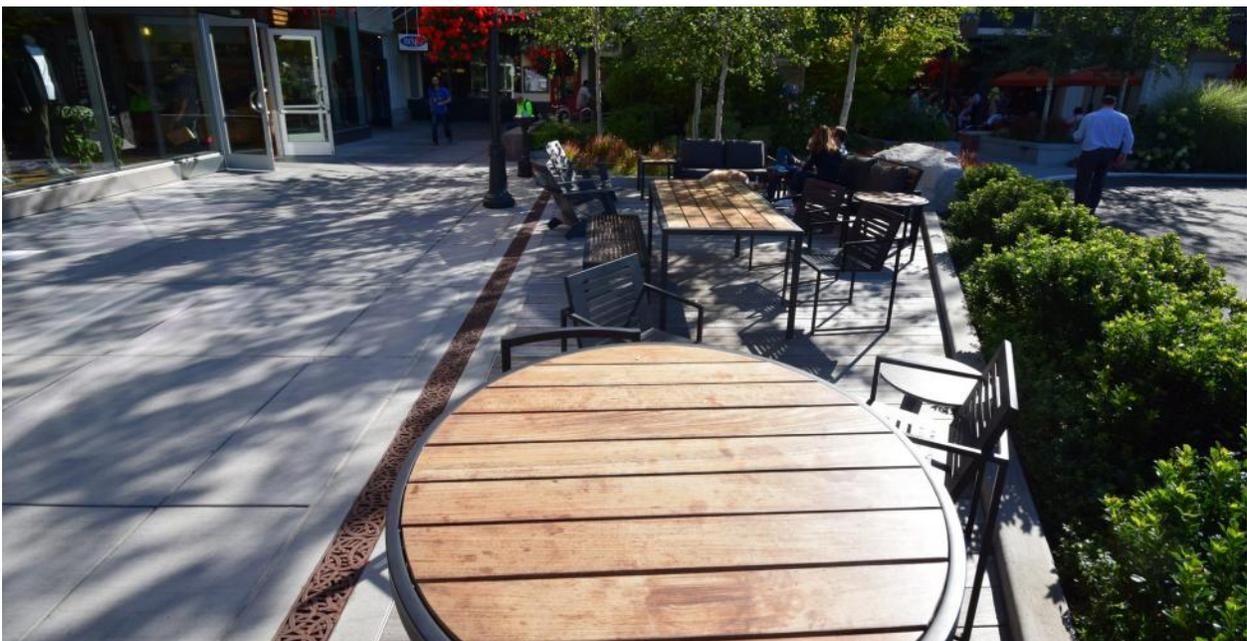
Intent: Provide a friendly pedestrian environment by creating a variety of usable and interesting public and semi-public open spaces.

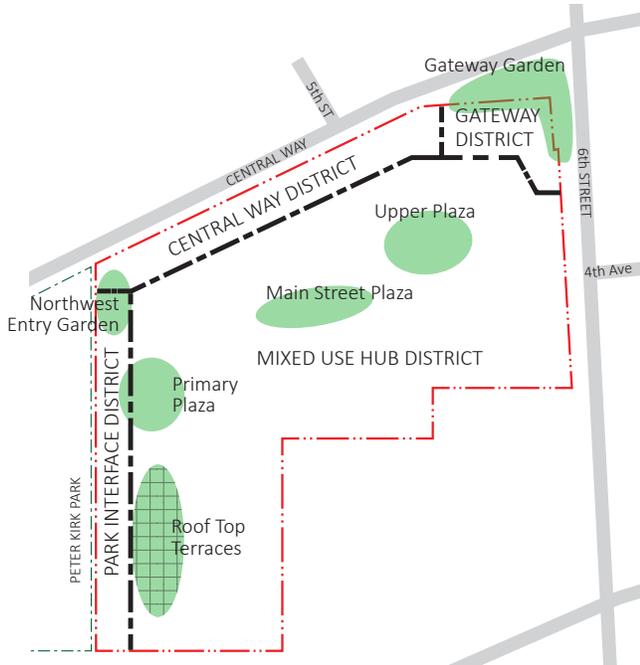
- a. Make plazas and courtyards comfortable for human activity and social interaction – standing, sitting, talking, eating, etc.
- b. Define and contain outdoor spaces through a combination of building and landscape. Oversized spaces that lack containment are discouraged.
- c. Establish pedestrian pathways that link public spaces to other public spaces and streets. These should be clearly identifiable for easy wayfinding.

Public Spaces: plazas defined by pathways and buildings include amenities such as water features, sitting spaces, landscaping, and changes in materials, colors, and textures



Street bench, plantings, and recessed corner entry





Possible Organization of Pedestrian/Public Spaces as Related to Districts

- d. Plazas and courtyards should include the following:
 - planters and trees to break up space
 - seating, such as benches, tables, or low seating walls
 - special paving, such as integral colored/stained concrete, brick, or other unit pavers
 - specialty pedestrian scale bollards or other types of accent lighting
 - at least one of: public art and/or water feature
- e. Design spaces to allow for variety and individualization of temporary installations such as: lighting, banners, artwork, etc.

3. ENVIRONMENTAL CONSIDERATIONS

Intent: Optimize pedestrian comfort using natural environmental conditions. Promote a pedestrian- and bicycle-friendly atmosphere.

- a. Consider environmental conditions such as sun, shade, and prevailing winds when positioning courtyards and outdoor seating areas. Provide features and amenities to enhance pedestrian and bicycle access throughout the project.

4. PEDESTRIAN CONNECTIONS AND WAYFINDING

Intent: Create a network of safe, attractive, and identifiable linkages for pedestrians.

- a. Provide clearly defined pedestrian connections at locations specified in the Pedestrian Spaces and Street Classification sections.
- b. Provide graceful grade transitions - both physical and visual - between upper grade and lower grade levels through the use of: landscaping, terraced planters, overlooking balconies, wide and inviting stairways, and other pedestrian connections.



Plaza with special paving, seating, planters



Pedestrian and bicycle amenities (left); Wayfinding signage and clearly defined pedestrian connections (center and right)

KIRKLAND PARKPLACE MIXED-USE DEVELOPMENT: MASTER PLAN & DESIGN GUIDELINES

5. LIGHTING

Intent: *Ensure that lighting contributes to the character of the project, provides personal safety, and does not disturb adjacent developments and residences.*

- a. Use city-approved fixtures for street lighting along the city streets.
- b. Lighting elements throughout the project and on adjoining rights of way should be coordinated, including public open spaces, accent lighting, and streets.
- c. Accent lighting along public right-of-way should be soft in character and enrich the pedestrian street life.
- d. Accent lighting within the central pedestrian space should be congruous with the character of the project and with the arts and pedestrian space commitments. (See Section 9.)
- e. Lighting should include non-glaring design, such as cut-off fixtures that avoid light spilling over onto other properties.
- f. Flood lighting of entire building facades is discouraged.
- g. Lighting on upper levels should be sensitive to Peter Kirk Park, residences, and drivers.



Integrated lighting enhances architectural character and provides pedestrian safety

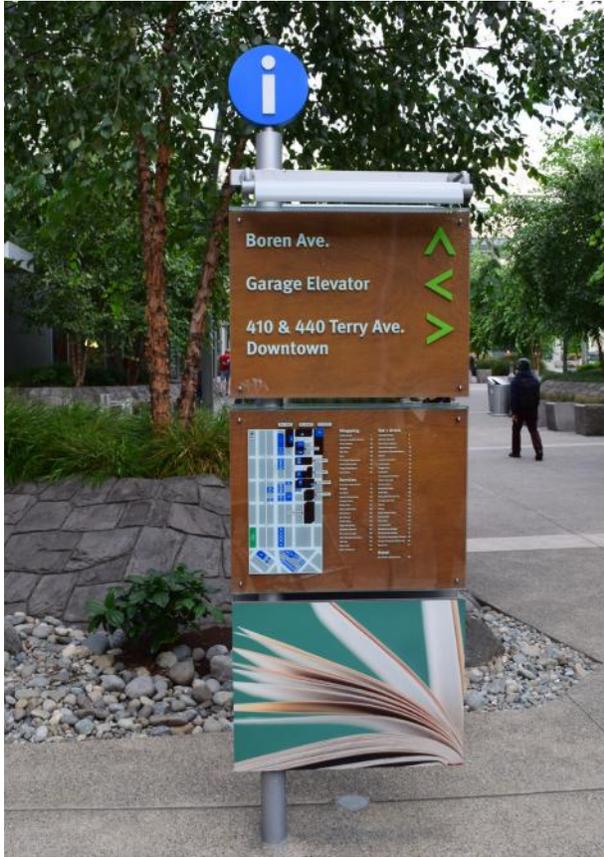
6. SCREENING OF TRASH AND SERVICE AREAS

Intent: *To screen trash and service areas from public view.*

- a. All service, loading, and trash collection areas shall be screened by a combination of planting and architectural treatment similar to the design of the adjacent building.
- b. Avoid wherever possible locating service, loading, and trash collection facilities in pedestrian-oriented areas.

Architectural and landscape elements provide screening





7. SIGNS

Create a Master Sign Plan that is in keeping with the following design objectives:

Intent: Create signs that are creative, engaging, and effective for a variety of user groups and respond to a variety of spaces.

- a. Signs should be complementary and integrated with the unique character of the specific areas or buildings where they are located.
- b. Signs should be high quality and consistent with the contemporary urban character of comparable developments in similar regions.
- c. The design of buildings should identify locations, sizes, and general design for future signs.
- d. The Master Sign Plan should include a hierarchy of elements based on function, such as:
 - site signs for entries, wayfinding, Parkplace identity
 - building signs for addressing and landmarking
 - tenant signs to encourage expressive individualization



A hierarchy of sign functions: site signs for entry and wayfinding (above), building signs for landmarking (below left), and tenant signs that express individual character (below center³ and right)



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BUILDING DESIGN

1. ORIENTATION TO THE STREET

Intent: *Ensure that buildings contribute to the liveliness of Parkplace's public spaces, and overall community character.*

The following design treatments should apply to areas where retail frontages occur:

- a. Streets and public spaces should be enlivened by storefronts, windows, merchandise and other activity. Buildings should be designed with frequent entrances to encourage multi-tenant occupancy and walk-in traffic.
- b. Ground level retail heights should be a minimum of 14 feet in height.
- c. Entrances: Principal building entry should be visible from internal or external streets and public space. Entries should be marked by large entry doors and/or canopy/portico/overhang.
- d. Transparency: To help provide a visual connection between activities, ground floor facades should provide:
 - windows of clear vision glass (i.e. transparent) beginning no higher than 2' above grade to at least 10' above grade,
 - 60% minimum of facade length along Central Way, and the internal Main Street, should provide transparency,
 - 50% minimum of retail facade length along access streets from Central Way to the site should provide transparency.
- e. Weather Protection: To provide pedestrians cover from weather, canopies or awnings should be:
 - a minimum of 5 feet in width unless in conflict with vehicles,
 - placed along at least 75% of facades of retail frontages, and constructed of durable materials,
 - allowed to vary in design,
 - encouraged to have continuity, minimizing gaps.



Retail frontages with wide sidewalks, transparency, visible entries, and weather protection



Articulation, massing, and diversity to maintain a pedestrian scale.
(middle: 4)

2. MASSING/ARTICULATION

Intent: Create a variety of form and massing through articulation and use of materials to maintain a pedestrian scale.

- a. In general, break down the scale and massing of larger buildings into smaller and varied volumes.
- b. All building faces should be responsive to the context of the surrounding environment and neighboring buildings.
- c. Design all sides of the building with care (i.e. there should be no “backside” of a building.)
- d. Buildings should distinguish a “base” using articulation and materials. Include regulating lines and rhythms to create a pedestrian-scaled environment.
- e. Provide clear pattern of building openings. Windows, balconies, and bays should unify a building’s street wall and add considerably to a facade’s three-dimensional quality.
- f. Ribbon windows and extensive use of mirrored glass are discouraged.
- g. Employ major architectural expressions into the facade, roof form, massing, and orientation, such as tower forms, oversized windows, and entrances to demarcate gateways and intersections. Strong corner massing can function as a visual anchor at key locations within the project area.
- h. Building modulation should be employed to break up long facades and create a visual interest unique to each building in the project. The type of modulation should be determined by the overall design concept for each building, using dimensions from window sizes, column spacing, rain screen paneling, etc. to determine a distinct design solution.
- i. Roof Silhouettes: Express roofs in varied ways. Consider potential views of roof tops from adjacent buildings. Avoid monotonous design.
- j. Locate and/or screen rooftop equipment so that it is not visible from public spaces. Integrate rooftop screening into building’s form.

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3. BLANK WALL TREATMENTS

Intent: Reduce the visual impact of blank walls by providing visual interest.

- a. Although blank walls are generally not encouraged along public streets and pedestrian spaces, there may be a few occasions in which they are necessary for functional purposes. Any blank walls longer than 20 feet should incorporate two or more of the following to provide visual interest:
 - vegetation, such as trees, shrubs, ground cover and or vines adjacent to the wall surface
 - artwork, such as bas-relief sculpture, murals, or trellis structures
 - seating area with special paving and planting
 - architectural detailing, reveals, contrasting materials, or other special visual interest



4. ENCOURAGE HIGH-QUALITY DESIGN

Intent: Ensure that all buildings in the project area are constructed as a quality addition to the Kirkland Community.

- a. Exterior architectural design and building materials should exhibit permanence and quality appropriate to Kirkland's urban setting.



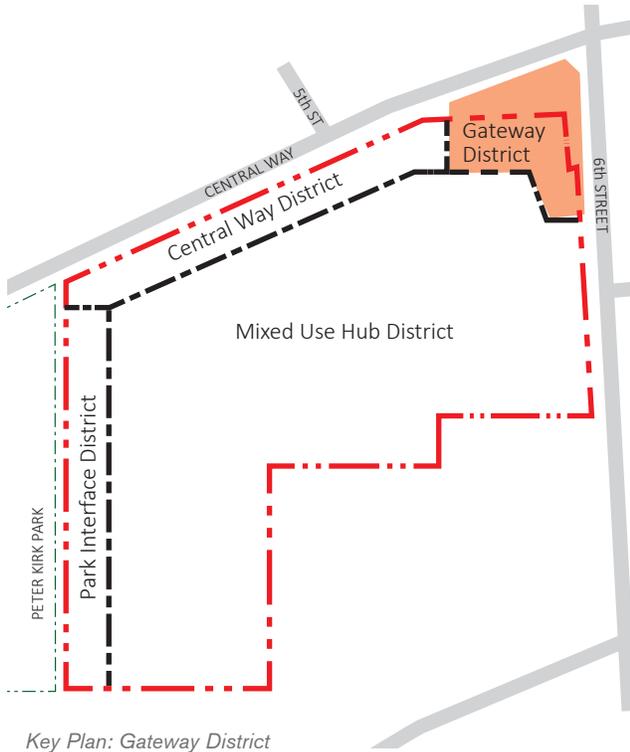
5. BUILDING DIVERSITY

Intent: Ensure that buildings in the project are distinct and respond to the unique character of their specific function and location.

- a. Buildings should be designed to integrate with each other, while demonstrating architectural diversity. Buildings should be responsive to each specific district and its site conditions.
- b. Materials should be selected to integrate with each other and to help provide a richness of architectural diversity.
- c. Windows should incorporate variation of patterning between buildings.



Vegetation, art, and screening provide visual interest at blank walls (center image ⁵)



Key Plan: Gateway District

13. Design Guidelines: District-Specific

A. GATEWAY DISTRICT

Intent: Create a welcoming feature to Parkplace and to downtown Kirkland. This area should create an inviting entryway that is representative of the community through the use of art, landscape, and architecture.

SITE PLANNING

1. Incorporation of Triangular Lot “Gateway Garden”: Incorporate the northeast triangular lot (excess right-of-way) into the project design to create a distinct gateway entrance that is integrated with the Parkplace development. Include:
 - a. Public Access: Public access into the site should be visible and accessible from the corner of 6th Street and Central Way.
 - b. Hardscape/Vegetation: Paving and landscaping materials should identify pedestrian spaces and access.
 - c. Trees and Other Planting: Landscaping should be of appropriate scale and species to make a significant gateway gesture. Trees should be selected to provide visibility of businesses and maintained to encourage proper growth and height.
 - d. Signage (downtown entry): Incorporate wayfinding signage directing visitors to Downtown, Peter Kirk Park, Waterfront/Marina, City Hall, and Civic District.
2. Public Space Connecting to Triangular Lot: Design of additional public space should be integrated with the triangular lot to provide a congruous pedestrian environment.
 - a. Public Access: Connect pedestrian access to the gateway garden, adjacent streets, and public open spaces.
 - b. Hardscape/Vegetation: Paving and landscaping materials should identify pedestrian spaces and access.
 - c. Seating: Incorporate seating along pedestrian pathways and gathering spaces.
 - d. Artwork: Incorporate art in an appropriate scale to distinguish the significance of this corner.



Distinct corner treatments: provide identity for the development and integrate pedestrian hardscape, landscaping, seating, and art

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3. Pedestrian Connection: Create a pedestrian connection from the corner of 6th and Central into the heart of the project. (See Section 10.A.) This connection will include the following:
 - a. Pedestrian weather protection
 - b. public connection from 6th to the interior of the site open during regular operating hours
 - c. pedestrian lighting
 - d. seating

and may include:

 - e. enclosed public space
 - f. retail/restaurant uses
 - g. covered play/activity space
4. Buildings should be separated from or differentiated from each other at this corner so that they are not perceived as one building.

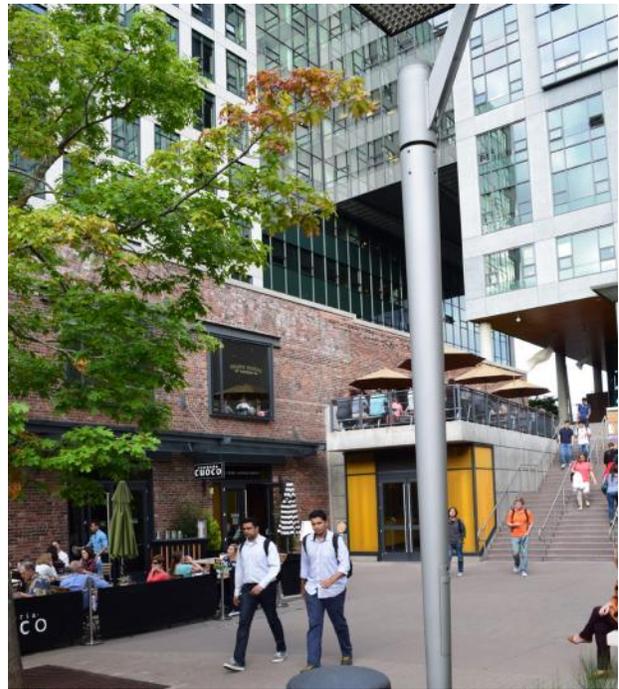
BUILDING DESIGN: BUILDING AS GATEWAY

1. Ground Level Treatment

- a. Setbacks from Streets - The ground floor levels of the corner building should be permitted to set back to allow for cut away view and obvious pedestrian connection into the site.
- b. Active and Inviting - Design for an engaging pedestrian experience along ground floor of the building.
- c. Details Visible at Different Movement Speeds - Incorporate details in the building along the corner that bring visual interest at the pedestrian level, as well as for vehicular traffic entering Kirkland.

2. Upper Levels

- a. Change of Expression/Material Choices: A clear visual division between upper and lower floors should be incorporated through a change in materials, colors, and forms.
- b. Modulation: Building form modulation should be incorporated to decrease the apparent bulk of the building facade along Central Way.
- c. Top Floor/Roof Edge: should have a distinct profile against the sky through elements such as projections, overhangs, cornices, step backs, trellises, changes in material, or other elements.
- d. Accent Lighting: The innovative use of accent lighting incorporated into the building facade is encouraged. Lighting should include non-glaring design solutions such as cut off fixtures that avoid light spilling over onto other properties. Flood lighting of entire building facades is discouraged.



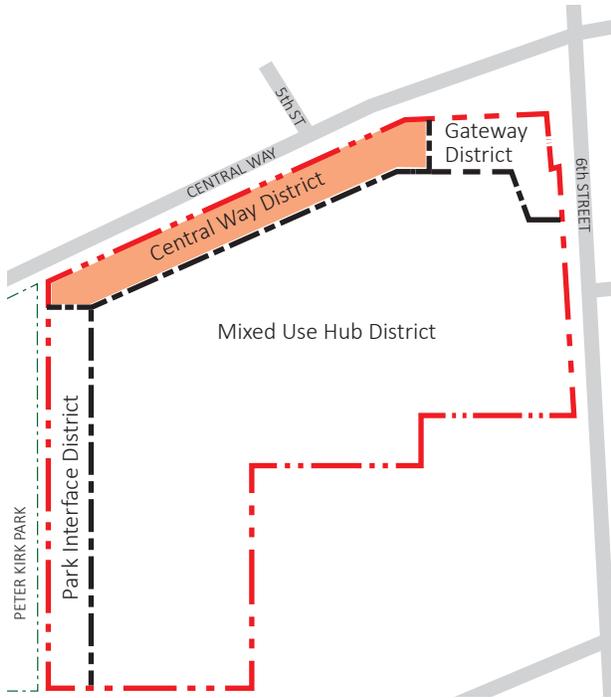
Building modulation, clear visual distinction between upper and lower floors, and details visible at different speeds



Ground floors set back to provide pedestrian connection to site



Top floor/roof edge provides distinctive profile⁶



Key Plan: Central Way District

B. CENTRAL WAY DISTRICT

Intent: Respond to Central Way as a major arterial linking downtown Kirkland with areas east and beyond. Parkplace must take advantage of this traffic volume to help create a multi-functioning, pedestrian-scale street that brings visual activity to the street edge.

SITE PLANNING

1. Encourage connections and activate the street edge by incorporating:
 - on-street parking along Central Way
 - buildings located up to the edge of the sidewalk
 - visibility into buildings in order to engage pedestrian interest
 - generous sidewalk amenity zone (trees, lights)
 - street tree selection and spacing that provide visual continuity, buffer pedestrians from the busy street, and allow visibility of retail
 - pedestrian signage
2. Reduce the length of street wall by pulling back portions of the building at ground level from the street edge in key locations provided street continuity is not interrupted.
3. Include a pedestrian-only connection from Central Way into the interior of the project. Pedestrian access along this route should include pedestrian-scaled lighting and a clear connection to the streetscape/plaza space on the opposite side.
4. Activate building corners with visibility into retail and/or other inviting design features, as denoted on *Organization of Uses* diagram (page 10).



Pedestrian-only connection from Central Way to interior streetscape/plaza on opposite side



Building corners articulated with glazing, canopies, and special paving

BUILDING DESIGN

1. Reduce apparent bulk of buildings along Central Way at pedestrian level. Provide a two story pedestrian pass-through in buildings fronting Central Way. The pass-through should be of sufficient size to provide views into the “main street” retail, and provide articulation in the building mass at grade.
2. At the upper levels, reduce apparent building facade length by providing breaks in the building facade or shifts in building mass.
3. Set back upper floors from the base at the western portion of the office building fronting Central Way.



Upper levels set back from base at western edge of office building; ample glazing and canopies enhance pedestrian experience⁸



Two-story pedestrian pass-through to promote physical and visual connections, and to reduce apparent building bulk at grade level



Breaks in building mass reduce apparent bulk of building⁹

C. PARK INTERFACE DISTRICT

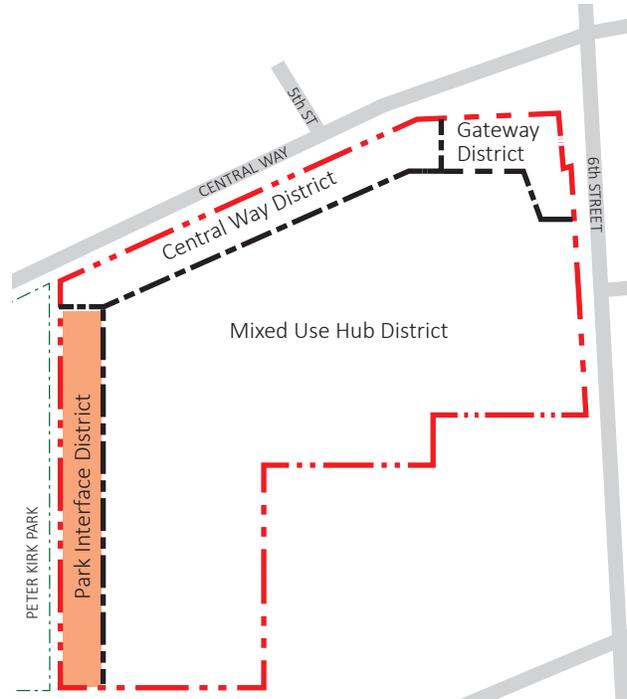
Intent: Create a strong connection from the park and downtown core that allows for clear pedestrian flow to and into the site by incorporating engaging building frontages, plazas, gardens, and other design treatments.

SITE PLANNING

1. Incorporate ample landscaping and distinctive lighting.
2. Incorporate raised crosswalks 20' minimum in width and special paving to promote pedestrian priority along the north-south street bordering the park.
3. Encourage retail spill-out spaces and landscaped courtyards along the building edge. Bring the “in-door” out and “outdoor” in by spilling retail spaces onto the sidewalk and creating small gathering spaces along building edges.
4. Create a visual barrier for drivers between the drive lane and pedestrian walkway along the Peter Kirk Park edge using one or more elements such as: plantings, bollards, small seating walls, stone artwork, etc.

BUILDING DESIGN

1. Buildings shall address park and street by incorporating:
 - terraces and balconies
 - entrances to retail along promenade
 - greater transparency at ground floor or planting zone and/or canopy at edge of buildings where transparency is not feasible, such as theater facades.
 - street front courtyards
 - retail spill-out spaces
2. Where feasible, provide rooftop terraces on lower roof levels as gathering spaces that include amenities such as:
 - seating
 - landscaping
 - canopies or coverings for weather protection
 - public access open during regular operating hours
 - retail/food service where appropriate



Key Plan: Park Interface District



Safe, clearly marked, pedestrian-friendly crosswalks



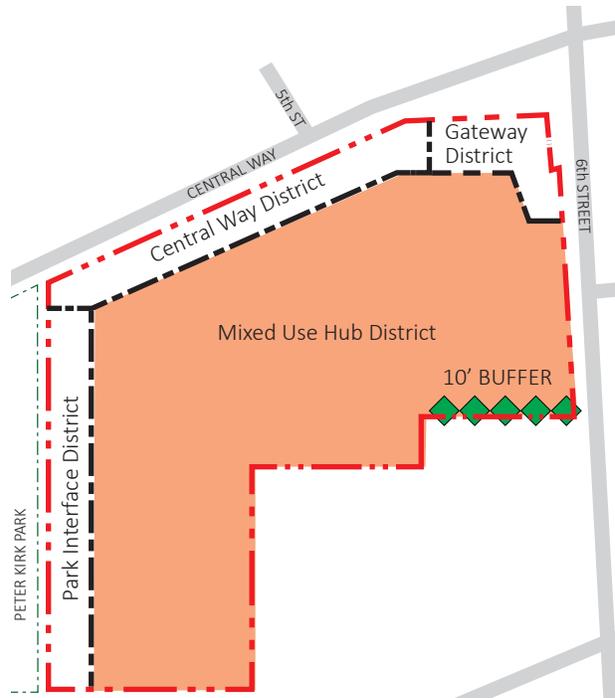
Pedestrian-oriented park interface: trees, clear markings, ground floor retail, balconies¹⁰

D. MIXED USE HUB

Intent: To establish a vibrant Mixed Use Hub with activated public space and retail/window shopping experience with a mix of uses, both connected to and overlooking the Main Street plaza, primary plaza, and Peter Kirk Park.

SITE PLANNING

- The plazas should be integrated visually and physically with their surroundings, and should provide significant gathering and activity spaces by incorporating the following:
 - special paving
 - water feature(s)
 - special landscaping
 - seating: covered and open
 - distinct lighting
 - access to sunlight
 - accommodations for concerts/performances
- Plazas should be supported as important activity spaces by surrounding them with active public-oriented amenities such as ground floor retail, restaurants, and cafes.
- Locate plazas at or near street grade to promote physical and visual connection to the street and adjacent buildings and their entrances.
- Design outdoor space with safety in mind; public plazas should promote visibility from the street and provide architecturally compatible lighting to enhance night time security
- A ten foot permanent landscaped edge along the southeast property line adjacent to residential uses should be incorporated within the street design. (See diagram at right.)
- The district should also consider providing:
 - small retail pavilion(s)
 - children's interactive feature
- A pedestrian connection on the southeastern portion of the site should be provided and include:
 - through public 24-hour access
 - connection to Peter Kirk Park
 - pedestrian weather protection and wayfinding signs.



Key Plan: Mixed Use Hub District showing buffer at southeast property line



Pedestrian courtyards framed by retail use below:11

BUILDING DESIGN

1. Lower level facades with predominantly retail uses should locate entrances at the sidewalk or edge of public space to frame pedestrian spaces in key locations.
2. Where feasible, provide rooftop terraces on lower roof levels as gathering spaces that include such amenities as:
 - seating
 - landscaping
 - canopies or coverings for weather protection
 - public access open during regular operating hours
 - retail/food service locations
3. In order to maximize the amount of sunlight in the primary plaza, buildings to the south should be contained under a line at a 41 degree angle measured from the center of the plaza.
4. Buildings located in the southern most portion of the site should provide generous and substantial modulation in response to their proximity to neighboring buildings including:
 - creating varied edges and visual interest on long and tall buildings
 - employing modulation to visually break up long facades
 - providing patterns of windows, bays, and/or balconies that emphasize changes in modulation



Plazas providing significant gathering and activity space, framed by buildings^{above:12, below:13}

Notes

IMAGE CREDITS

The following sources were used for end-noted images. All other images and illustrations are provided by CollinsWoerman.

1. Kirkland, WA. Map. *Google Maps*. Google, 6 Aug 2014. Web. 6 Aug 2014.
2. VA, Brett. *Outbuilding bike parking Kirkland WA*. 30 Jan 2010, Kirkland, WA, in *Flickr*. https://www.flickr.com/photos/smart_growth/4575869318/in/set-72157623983604822/
3. La Citta Vita. *Courtyard Shopping in Berlin*. 16 Jan 2011, in *Flickr*. <https://www.flickr.com/photos/la-citta-vita/5852199389/>
4. [from 2008/LMN version]
5. La Citta Vita. *Green Wall*. 27 Feb 2012, in *Flickr*. <https://www.flickr.com/photos/la-citta-vita/7802506458/in/photolist-c6gcQA-8fZ64K-8fZ5FT-cTtTkq-aHZJqg>
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11. [credit in process]
12. The Jerde Partnership International. 1999. *You Are Here*. London: Phaidon Press Limited.
13. [credit in process]

DRAFT KIRKLAND PARKPLACE MIXED-USE DEVELOPMENT: MASTER PLAN & DESIGN GUIDELINES

CREDITS IN PROCESS

November 26, 2014

Dear Planning Commission members,

I recently met with Bill Pollard to discuss the Park Place project.

It was suggested by one Planning Commissioner that I give my most important points at the beginning of my letters.

My most important point is that the Commissioners need to get a look at the lease agreement between the past owner of the Park Place property and the QFC Corporation (now owned by Kroger's). This is the lynchpin to the whole development plan of the property. Because of that legal agreement there are a great many limitations placed on the current developer. In order for you to make a wise well informed decision you need to know the specifics of the agreement. Everything hangs on it.

Here are my less important comments which I may have said in some other way before since I tend to rethink things once new information presents itself:

It seems to me that the Commissioners have to take into account what other development projects are in the planning stage as well as those that are already being built around Kirkland at the moment. I'm sure you are very well aware of this fact but as a citizen I feel it is an overwhelming amount of information for me to look at. The citizens trust you to see the bigger picture.

What I have had to grapple with is that the situation is very different than in was in 2008 when various property owners were waiting to see what happened at the Park Place site first before deciding on their own course of action. I mention this because I am rethinking my stand on residential development in and around the CBD area. I now have mixed feeling about a residential component. When I look at how Redmond has grown I can't decide if there are lessons for us to learn on what works and what doesn't. Certain aspects of Redmond Town Center have not been as successful as were originally envisioned. It makes me wonder if that was partly due to the fact that there wasn't residential development included on that site. Maybe that is part of the reason the grocery store was not successful. In the areas where the housing density has increased in downtown Redmond, shopping seems to be brisker than it was in the past (judging by parking lot usage). I'm not a planner so I have no idea of whether there is a synergistic relationship in an urban mixed-use development between the various uses that the commissioners need to give a certain weight to during their discussion.

Here are the questions I have been musing on. Does making the Park Place Development a work- live-shop environment support the other retail in the greater downtown area? Do we have to have all three components to make the retail at Park Place successful? Does it really matter whether the QFC and retail shops are on the north, east, south or west side of the Park Place property in order for shoppers to want to walk from east of the park to west of the park? What encourages people to do so now? Is it partly due to the fact that there is available free parking at the library garage? Or does the location of the transit center have something to do with it? Is it partly due to the fact that you can see some of the shops both east and west from the park, library and transit center? I will admit that I drive to the library to pick up my bag of books and then drive to the QFC to buy my groceries. I don't want to carry heavy bags very far especially in the rain. The park will always separate the middle of Kirkland. Do we want to see the downtown as an urban version of a neighborhood business center and Totem Lake area as the destination retail and office center? Many citizens have said that they want more growth in Totem Lake

and less growth in Kirkland. Here is an example from someone who contributed frequently to the Comprehensive Plan Update public conversation:

Shift terminology or “how we think” of Downtown Kirkland from a business center to more of an “Old Town” feel. Emphasis on living, dining, tourism/Marina. Our Business center, our Economic engine, our Center of Kirkland is Totem Lake Urban Center and Yarrow Bay Business District. That is where we work. ---Lisa McConnell

Many people feel this way. I’m sure you have already studied this carefully but here is the 2035 vision statement from the Moss Bay/Lakeview group discussions:

Where and how should growth occur in the City? (Dispersed vs. concentrated?)

- 1/3 Central Business District, 2/3 Totem Lake
- 1/4 CBD/Moss, 3/4 Totem Lake
- Rename Central Business District to Waterfront District
- Reutilize existing properties
- Redevelop Houghton shopping center - allow to go up
- May need another shopping center to be within walking distance
- Home occupation and ADU rules need to meet multigenerational families; Condo covenants limit these
- Encourage home based business; – fewer car trips
- Totem Lake
- Jobs walking distance from housing
- NE of Big O Tires
- CBD/Totem Lake/BT Shopping Ctr
- Around neighborhood commercial centers
- Yarrow Bay
- Encourage tourism and retail in the CBD
- Develop Park Place like University Village
- Grow not like Bellevue
- NE 85th St
- Not in Kirkland waterfront; should retain waterfront-related activities such as restaurants, farmers markets, music
- Totem Lake should provide big box stores, taller office where there are roads & transit
- Bridle Trails needs more activity (although surrounding roads are too narrow)
- Roads need to be designed to accommodate growth
- South Kirkland Park & Ride - low income housing good idea near parking & transit
- Meet growth with current zoning
- Keep growth in scale with neighborhood
- City mistake – Portsmouth building too tall; evaluate existing zoning to make lower buildings and reduce density

- Encourage grocery stores to stay

As you see, some people feel tourism is an important component in supporting retail in the downtown area where we draw visitors from all over the world because we have a cute town with several great parks as well as a marina. Personally I was disappointed with the fact that the hotel and conference center were removed from the Park Place plan. It guaranteed more tourism for Kirkland. And a conference center could benefit the businesses, the locals, and the tourists. It is a great asset to have a space for weddings, community meetings and business events. In a way, a hotel is a residential development that supports restaurants and shops because tourists don't usually cook in their rooms.

My one last question is: what kind of entertainment do we need in Kirkland? What does 'Live-Work-Play' really mean? We already have a marina, several parks including a baseball field, a performance center, and a library. Kirkland has charity races and festivals all year round. There are great entertainment venues and a shopping mall in Bellevue 4.2 miles from Kirkland library. I don't believe we can compete with what Bellevue has to offer as a destination retail/entertainment center. Do we need shops open until 10pm or can we keep some of our suburban family feel? I vote for having shops close before 8 to encourage people to go home at night and spend time with family. This is an important concept to think about when trying to envision retail at Park Place. Many of us don't want cars driving along our streets from all over the region late at night. We don't have to be a late night party town to have successful retail. Downtown Kirkland needs to have a sense of balance as well as a welcoming community spirit. Kirkland is a great place to raise a family. No matter how much development the city needs to accommodate for future growth I hope we can always keep the sense that we are a welcoming multigenerational place to live.

Sincerely,
Margaret Bull